



July 18, 2012

Dr. David Crass, Director,
Deputy State Historic Preservation Officer,
and State Archaeologist
Historic Preservation Division/DNR
254 Washington Street, SW
Ground Level
Atlanta, GA 30334

RE: Project CSHPP-0007-00(561), Clarke County, P.I.#0007561 and HP #080728-001:
Section 106 Compliance - Assessment of Effects and Memorandum of Agreement.

Dear Dr. Crass:

Project CSHPP-0007-00(561), Clarke County consists of the proposed conversion of approximately one mile of the abandoned Georgia Railroad line into a pedestrian trail. Please find enclosed the Section 106 documentation for the Tweedell House, the Smith House, the Fuller House, the Wilkerson Street Historic District, the Inglewood Avenue Millhouse District, and the Georgia Railroad Corridor and Contributing Properties, National Register eligible historic properties located within the proposed project's area of potential effects (APE).

The enclosed documentation was prepared by Summer Ciomek of New South Associates of Stone Mountain, Georgia, for use in compliance with Section 106 of the National Historic Preservation Act of 1966 and subsequent amendments. The Department has reviewed and concurs with this report. The documentation consists of the Assessment of Effects and copies of the Survey Report Property Information Forms for the resources. The Survey Report was previously submitted to your office, to the Federal Highway Administration (FHWA) and to all other consulting parties in the Section 106 process for this project. In compliance with 36 CFR 800.4(c)(2), the aforementioned properties were considered eligible National Register properties by the FHWA and the SHPO.

A determination of No Effect has been made for the Tweedell House, the Smith House, the Fuller House, and the Wilkerson Street Historic District. A determination of No Adverse Effect has been made for the Inglewood Avenue Millhouse District. A determination of Adverse Effect has been made for the Georgia Railroad Corridor and Contributing Properties. Also enclosed is a draft Memorandum of Agreement (MOA) for the adversely affected Georgia Railroad Corridor and Contributing Properties. Please review the draft MOA at this time and provide comments on or concurrence with the proposed stipulations. Upon completion of coordination with the Advisory Council on Historic Preservation, a final draft of the MOA will be submitted to your office for review and signature.

In accordance with 23 CFR Part 774 (Sections 774.3(b) and 774.17) of the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU), Pub. L. 109-59, the FHWA and GDOT intend to make a *de minimis* finding based on your concurrence with the Section 106 determination of "No Adverse Effect" for the resource listed in the table below. The Department

requests the SHPO's acknowledgement of this *de minimis* finding and also requests that the SHPO's acknowledgement of *de minimis* includes any potential use of additional land from this resource as long as the change is coordinated with the SHPO, and the effect remains no adverse per the 2003 Memorandum of Understanding.

Name of Resource	Nature of Impact	Total Acreage of Eligible Boundary	Total Acreage of Land Use Within Eligible Boundary
Inglewood Avenue Mill House Historic District	The abandoned Georgia Railroad bed would be paved to create a pedestrian/bicycle trail.	12.15 acres	0.6 acre

A copy of the enclosed documentation has been forwarded to the FHWA and to all other consulting parties. Please notify this office of the scheduled consultation date, if any, with the FHWA to discuss this project. Please respond to this documentation within thirty (30) days of receiving this information.

If additional information is required, please contact Chad Carlson (404-631-1406 or ccarlson@dot.ga.gov) or Sandy Lawrence (404-631-1150 or slawrence@dot.ga.gov) of the Office of Environmental Services. We appreciate your assistance in this matter.

Sincerely,



Glenn Bowman, P.E.
State Environmental Administrator

GB/cbc
Enclosure

cc: Rodney N. Barry, P.E., FHWA, w/attachment (Attn: Kelly Wade)
Northeast Georgia Regional Commission, w/attachment
Lisa Deaton, GDOT NEPA
Justin Lott, GDOT TE Project Manager
Summer Ciomek, New South Associates

**MEMORANDUM OF AGREEMENT
BETWEEN
FEDERAL HIGHWAY ADMINISTRATION
GEORGIA DEPARTMENT OF TRANSPORTATION
AND
GEORGIA DEPARTMENT OF NATURAL RESOURCES
HISTORIC PRESERVATION DIVISION
AND
ATHENS-CLARKE COUNTY UNIFIED GOVERNMENT
FOR
GDOT PROJECT CSHPP-0007-00(561)
PI# 0007561
CLARKE COUNTY, GEORGIA**

WHEREAS, the Federal Highway Administration (FHWA) has determined that Georgia Department of Transportation (GDOT) Project CSHPP-0007-00(561), PI# 0007561, Clarke County, Georgia, will have an effect upon one property eligible for inclusion in the National Register of Historic Places (NRHP), and has consulted with the Georgia State Historic Preservation Officer (SHPO) and the Northeast Georgia Regional Commission pursuant to 36 CFR Part 800, regulations implementing Section 106 of the National Historic Preservation Act (16 U.S.C. 470F); [and Section 110(f) of the same Act (16 U.S.C. 470h-2(f)] and,

WHEREAS, the project is located in Downtown Athens and traverses approximately one mile. The entire project is within Athens-Clarke County. The project generally encompasses the use of an abandoned railroad bed to create a pedestrian/bicycle trail starting from the intersection of Old Winterville Road and ultimately ending on East Broad Street. The project beginning is at the location of the proposed Athens Park and Ride Lot and ultimately ends within walking distance of the Athens Multi-Modal Transportation Center. The project includes a tie-in point to the existing Dudley Park greenspace and walking trails that provide interconnection to the North Oconee Greenway, between Trail Creek and the North Oconee River. The transportation facilities located at both ends of this project will allow accessibility to users outside of the neighboring community; and,

WHEREAS, six NRHP listed or eligible properties were identified in the project's area of potential effects; and,

WHEREAS, an Assessment of Effects was completed on July 5, 2012, for these resources with findings of No Effect for the Tweedell House, the Smith House, the Fuller House, and the Wilkerson Street Historic District; No Adverse Effect for the Inglewood Avenue Millhouse District; and Adverse Effect for the Georgia Railroad Corridor and Contributing Properties; and,

WHEREAS, if GDOT Project CSHPP-0007-00(561), PI# 0007561 is not implemented within seven (7) years of this agreement's ratification, this agreement shall be considered null and void, and the FHWA, if it chooses to continue with the undertaking, shall reconsider the terms of this agreement; and,

WHEREAS, if the SHPO should object within thirty (30) days to any plans provided for review pursuant to this Memorandum of Agreement (MOA), the FHWA shall consult with the SHPO to resolve the objection. If the FHWA determines that the objection cannot be resolved, the FHWA shall request further comments or recommendations of the Advisory Council on Historic Preservation (Council) concerning the dispute pursuant to 36 CFR Part 800. Any Council comment provided in response to such a request will be taken into account by the FHWA in accordance with 36 CFR Part 800 with reference only to the subject of the dispute. The FHWA's responsibility to carry out all actions under this MOA that are not the subject of the dispute will remain unchanged; and,

NOW, THEREFORE, the FHWA and the SHPO agree that the undertaking shall be implemented in accordance with the following stipulations in order to take into account the effect of the undertaking on the historic property.

STIPULATIONS

The FHWA will ensure the following measures are carried out:

- 1) Since the Trail Creek Trestle will not be maintained or rehabilitated, historical and photographic documentation of the trestle will be completed before it deteriorates further. This documentation will consist of the development of a brief architectural description and historical narrative of the trestle. The Trail Creek Trestle will be photographed with high-resolution digital photography per standards previously agreed upon between GDOT and SHPO in the Permanent Archival Record manual. The photography and accompanying documentation will be submitted to the SHPO for acceptance and retention.
- 2) Interpretive signage providing historical data about the Trail Creek Trestle will be produced and installed in conjunction with project implementation. This sign will be of comparable size, style, and materials as the existing interpretive panels within Dudley Park.

Execution of this MOA by the FHWA and the SHPO, its subsequent filing with the Council, and implementation of its terms, is evidence that the FHWA has afforded the Council an opportunity to comment on GDOT Project CSHP-0007-00(561), PI# 0007561, ClarkeCounty, Georgia, and its effects on historic properties, and that the FHWA has taken into account the effects of the undertaking on historic properties.

FEDERAL HIGHWAY ADMINISTRATION

BY: _____ DATE: _____
Rodney N. Barry, P.E., Division Administrator

GEORGIA STATE HISTORIC PRESERVATION OFFICER

BY: _____ DATE: _____
David Crass, Division Director, Deputy SHPO/State Archaeologist

GEORGIA DEPARTMENT OF TRANSPORTATION

BY: _____ DATE: _____
Glenn Bowman, P.E., State Environmental Administrator

ATHENS-CLARKE COUNTY GOVERNMENT

BY: _____ DATE: _____
J. Derek Doster, Project Administrator

ASSESSMENT OF EFFECTS

GDOT PROJECT CSHPP-0007-00(561), CLARKE COUNTY

P.I. # 0007561

HP #080728-001

FINDING OF NO EFFECT

The Tweedell House

The Smith House

The Fuller House

The Wilkerson Street Historic District

FINDING OF NO ADVERSE EFFECT

The Inglewood Avenue Millhouse District

FINDING OF ADVERSE EFFECT

The Georgia Railroad Corridor and Contributing Properties

July 5, 2012

This document has been produced for use in compliance with Section 106 of the National Historic Preservation Act of 1966 and subsequent amendments.

INTRODUCTION

This document has been prepared for use in completion of applicable Section 106 procedures in compliance with the National Historic Preservation Act (NHPA) of 1966 and subsequent amendments. As such, this document assesses the effects to historic properties identified within the area of potential effects (APE) of GDOT Project CSHPP-0007-00(561) Clarke, County.

The design of the proposed project was developed by GDOT engineering personnel who, as a standard procedure, include environmental parameters as a part of the initial investigations prior to laying out a proposed design. Basic data of the project area that was gathered and studied included aerial photography, topographic maps, traffic (existing and projected), previous studies, wetland inventory maps, soil survey maps, flood plain maps, and Georgia Department of Natural Resources' historic resource survey maps.

That data was used to delineate wetland or hydric soil boundaries, flood plains, parks and recreational facilities, known or suspected historic properties and archaeological sites, existing rights-of-way, possible UST/landfill/hazardous waste sites, and areas of possible endangered species habitat on the aerial photography prior to laying out an alignment. In addition, other "controls" such as churches, cemeteries, schools, hospitals, and any other noise sensitive areas were also identified.

Only at this point was the proposed alignment developed with every attempt being made to avoid sensitive ecological, historic and archaeological areas. In the event that avoidance was not possible, every attempt was made to minimize harm to such resources. The proposed alignment, once laid out, was then field checked and additional refinements were made to further minimize harm to both the natural and built environment.

NEED AND PURPOSE

The need for this project is to provide the Athens community with a vibrant, upscale pedestrian facility/destination area connecting two important transportation links, and at the same time preserve and enhance a piece of the community's history while providing the community with a recreation facility. A Citizens Advisory group from the local community has been working on making the trail a reality for a number of years, and this project has been identified as one of the projects in the Athens-Clarke County 2005 SPLOST Program. This project would also fulfill the need for connectivity in the overall Athens-Clarke County Pedestrian/Bicycle Corridor. This path would connect users to Dudley Park, an in-town park, and the North Oconee Greenway Trail, which connects downtown Athens and UGA (University of Georgia) to the Sandy Creek Nature Center (north of Athens).

The corridor currently consists of an abandoned railroad bed and the remains of two old railroad bridges. The concrete piers used to support the North Oconee Bridge over the North Oconee River remain along with remnants of the Trail Creek wood trestle. Both the North Oconee Bridge and Trail Creek Trestle were most likely constructed in 1883. The remaining North Oconee Bridge concrete piers were probably constructed post World War II. The age of the Trail Creek trestle timber remnants is varied, but the structure was effectively rebuilt since

World War II as the trestle timber was subject to regular and ongoing repair and replacement throughout its operating life.

The purpose of this corridor is three-fold: to be a pedestrian/multi-use/alternative transportation link between the proposed Athens Park and Ride facility (Project No. MTA00-T001-00(904)) and the Athens Multi-Modal Transportation Center; to encourage bicycle and pedestrian usage as an alternate mode of transportation; and to provide educational opportunities with interpretative signage that includes historic narratives about the area. It would establish a convenient accessible non-automotive alternative and high quality recreational experience for residents and visitors. The multi-use path would provide access to Dudley Park, an in-town Athens greenspace, that includes a connection via existing park walkways to the North Oconee River Greenway, an expansive trail way that follows the river.

The construction and completion of the Athens Multi-Modal Transportation Center was a major catalyst for the redevelopment of the area. This redevelopment has resulted in the construction of new office buildings and condominiums in the area. Currently there is no direct access from this area of Athens to the in-town Dudley Park. This trail would provide access to this park. It would also provide a link to a future Park and Ride facility, which is to be located on a major transportation corridor - U.S. 78 and the Georgia 10 Loop around the City of Athens.

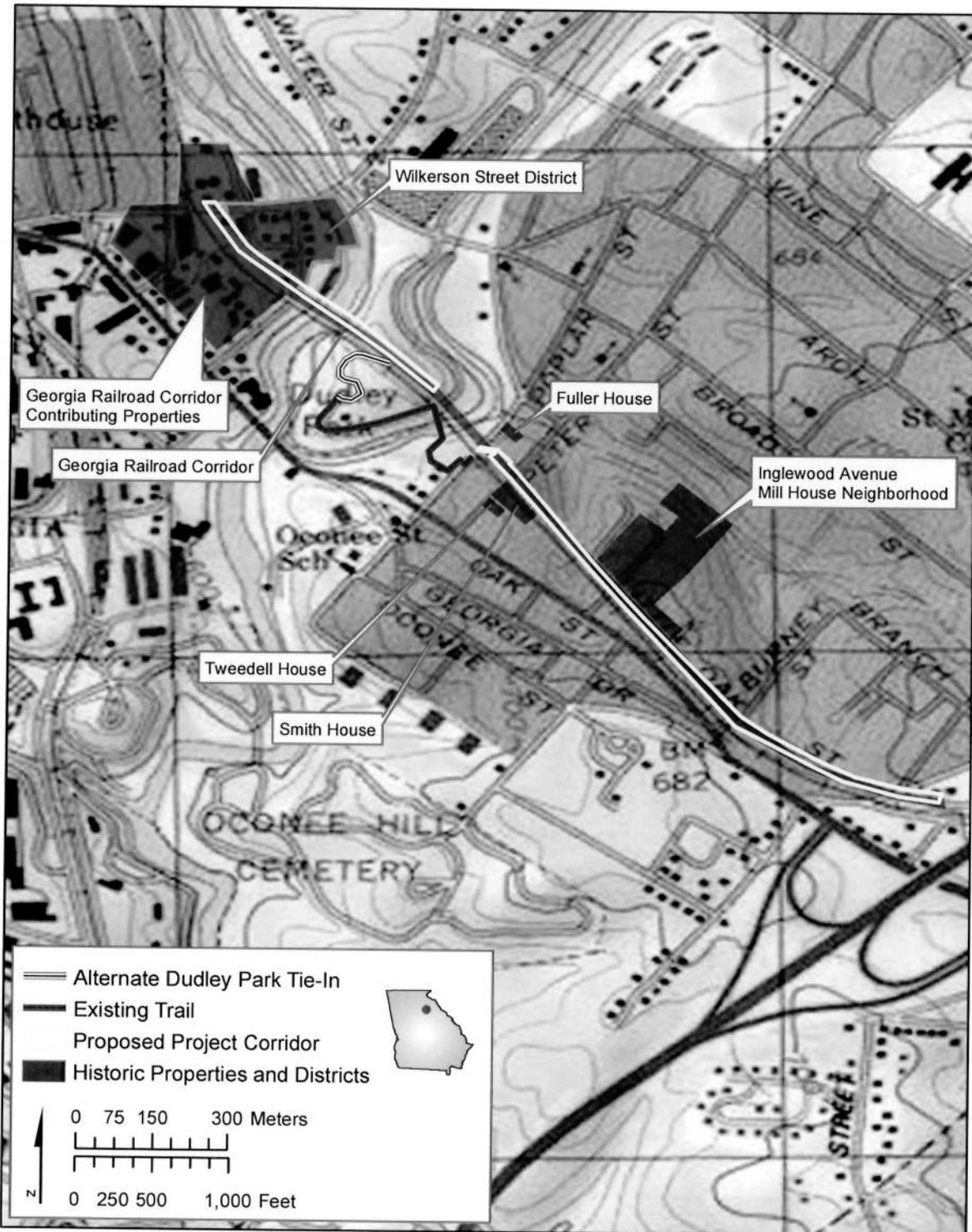
The ultimate goal of this project is the completion of construction of the full-length trail from the Park and Ride facility to the Multi-Modal Center. The availability of funding for the overall construction may require building the project in phases. The anticipated first phase would start at the project beginning at Old Winterville Road, proceed along the former railroad bed alignment to a new pedestrian bridge over Peter Street, and then tie in with an ADA access ramp to grade at Poplar Street with access to the entrance to Dudley Park. The anticipated second phase would include the construction of the trail from a tie in point at the existing walking trails within Dudley Park to the railroad right-of-way and end with the continuation of the trail west along the former railroad bed to the North Oconee River. The final phase of trail construction would be west from the North Oconee River, including the North Oconee River Bridge, to the end of the project at East Broad Street.

DESCRIPTION OF THE UNDERTAKING

GDOT Project CSHPP-0007-00(561) is federally funded. Therefore, Section 106 compliance is being processed through the Federal Highway Administration (FHWA).

Initially, the proposed project would consist of the renovation of approximately one mile of an abandoned railroad bed as a trail. The entire project is in Athens-Clarke County. The abandoned railroad bed extends from the intersection with Old Winterville Road and ends on East Broad Street. The trail would begin at grade from Old Winterville Road and follow along the abandoned railroad bed to Peter Street. A single span prefabricated pedestrian bridge structure would be used to cross Peter Street and then the path follows a short distance at grade to another single span prefabricated pedestrian bridge over Poplar Street. The existing structure crossing Trail Creek is a wooden trestle better known as the Murmur Trestle. The trestle once crossed Poplar Street but is no longer complete. The trestle would be inspected for structural soundness

Figure 1
Project Location Map
 GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: USGS Quadrangle Athens East, GA, 1976

and ability to support the trail. The trail then returns to grade for a short distance to the North Oconee River crossing. This abandoned section of the railroad contains existing piers of a bridge over the North Oconee River and Wilkerson Street. The project would include the design and construction of a pedestrian bridge utilizing the existing piers to span the North Oconee River and Wilkerson Street. The trail would then come back on grade for a short section before the end of the project at East Broad Street. The trail would be contained in 14-feet of right-of-way centered on the existing rail within the existing CSX Railroad right-of-way.

Since the time of the initial project description, the concept report and, therefore, the project description have been revised. Since the Trail Creek Trestle was found not to be structurally sound, it could not be used to carry the trail over Trail Creek. Due to the expense and liability involved, it was determined to be most prudent and feasible to reroute the trail around the trestle. In order to reroute the trail to avoid the trestle, the original trail alignment was replaced by the current alignment, Alternative B. The revised project description is as follows:

The project is located in Downtown Athens and traverses approximately one mile. The entire project is within Athens-Clarke County. The project generally encompasses the use of an abandoned railroad bed to create a pedestrian/bicycle trail starting from the intersection of Old Winterville Road and ultimately ending on East Broad Street. The following project description describes the trail from east to west.

The trail would begin at grade from Old Winterville Road and would traverse the existing abandoned railroad bed westward to Peter Street. A single span prefabricated pedestrian bridge structure would be used to cross Peter Street. The trail would continue along the abandoned railroad bed until it reaches the east side of Poplar Street where it would traverse to grade and connect to an existing sidewalk. The path of travel would follow the existing sidewalk system and crosswalk of Poplar Street for a short distance connecting to the existing walkway system into Dudley Park.

The path of travel would follow the existing walkway system through Dudley Park until a new section of trail is constructed to re-traverse from grade back to the abandoned railroad bed on the west side of Trail Creek. This proposed trail alignment and use of the existing walkway system through Dudley Park would avoid an existing historic structure crossing Trail Creek, the Trail Creek Trestle, which lies within a designated historic railroad corridor. Connection to the existing greenway and walkway systems would provide users another alternative to reach the Multi-Modal Transportation Center on East Broad Street.

The trail would continue along the abandoned railroad bed west of Trail Creek for a short distance prior to the North Oconee River crossing. The abandoned North Oconee River crossing contains existing piers of a bridge over the North Oconee River and Wilkerson Street. The project would include the design and construction of a pedestrian bridge utilizing the existing piers to span the North Oconee River and Wilkerson Street. The trail then returns to grade along the abandoned railroad bed west of the North Oconee River and Wilkerson Street until terminating at grade at East Broad Street, the end of the project.

Utilizing the existing structures and trails where feasible allows the minimization of environmental disturbance associated with the construct of new foundations and avoids constructing on/at the Trail Creek Trestle. This approach would also allow for faster installation, which would significantly shorten the construction time and cost.

The project beginning is at the location of the proposed Athens Park and Ride Lot and ultimately ends within walking distance of the Athens Multi-Modal Transportation Center. The project includes a tie-in point to the existing Dudley Park greenspace and walking trails that provide interconnection to the North Oconee Greenway, between Trail Creek and the North Oconee River. The transportation facilities located at both ends of this project would allow accessibility to users outside of the neighboring community.

The APE, as defined in 36 CFR 800.16(d), is the geographic area or areas within which an undertaking may directly or indirectly cause changes in the character or use of historic properties if any such properties exist. Based on the nature and the scope of the undertaking, the guidance in the GDOT/FHWA Cultural Resources Survey Guidelines and past experience with similar projects, the Department has evaluated and defined the APE for this proposed project. Because of the nature and scope of the undertaking, the APE is limited to the existing railroad right-of-way and viewshed of the proposed project, within which all construction and ground disturbing activity would be confined (Figure 1). No potential for indirect effects outside of the existing right-of-way and viewshed of the proposed project exists.

IDENTIFICATION OF HISTORIC PROPERTIES

Existing information on previously identified historic properties was checked to determine if any are located within the APE of this undertaking. This review included National Register-listed properties, proposed National Register nominations, National Historic Landmarks, and the updated Georgia Historic Bridge Survey (GHBS). The Department of Natural Resources (DNR) Clarke County survey dated 1974, and the City of Athens survey dated 1988, were consulted, and revealed no previously recorded historic properties. The state archaeological site files at the University of Georgia and existing survey reports were consulted to locate previously identified archaeological sites within the APE. No previously recorded archaeological sites were identified within the APE. Also topographic maps and aerial photography were reviewed to identify areas of high archaeological site potential within the APE.

Following the review of existing information on previously identified historic properties potential consulting parties in the Section 106 process were identified. In addition to the Georgia SHPO, other potential consulting parties were identified based on the nature of the undertaking and the guidance in the GDOT/FHWA Cultural Resource Survey Guidelines. The other potential consulting parties invited to participate in the Section 106 process were the Northeast Georgia RDC, the Clarke County Board of Commissioners, the Athens Historical Society, the Athens-Clarke County Library, and the Athens-Clarke County Historic Preservation Commission. In accordance with 36 CFR 800.2, these consulting parties were informed of our efforts to locate previously identified historic properties and the results of those efforts and were asked to provide information on any unidentified National Register-listed or potentially eligible

historic properties located within the proposed project's APE by Notifications dated July 25, 2008 and March 4, 2009 (Appendix A).

The Department also requested available information on past, present and future local developments or zoning plans that could result in indirect or cumulative impacts to historic properties. A response was received from the Georgia SHPO (HPD) by a letter dated August 05, 2008, and the Northeast Georgia RDC by a letter dated August 18, 2008 (Appendix A).

After reviewing any additional information received from consulting parties, field surveys and background research were conducted within the APE of the proposed project to identify any historic properties or archaeological sites eligible for listing in the National Register. The results of the field surveys and background research were summarized in a Survey Report and an Archaeological Assessment. That report was provided to all consulting parties participating in the Section 106 process for review and comment.

As a result of these identification efforts, six National Register-eligible historic properties were identified within the proposed project's APE (Figure 1). These historic properties are identified below in Table 1. No National Register-eligible archaeological sites were identified. The historic properties Survey Report was submitted to the SHPO and FHWA on June 19, 2009. The Archaeological Assessment was submitted to the SHPO and FHWA on June 17, 2010. In accordance with 36 CFR 800.4(c)(2), these properties were considered eligible for listing in the National Register by the FHWA and the SHPO.

Table 1. National Register-Eligible Historic Properties

Resource	Period of Significance	Type / Style
Inglewood Avenue Mill House District	1913-1916	Residential – Mill worker’s housing
Tweedell House	circa 1952	Saddle Bag
Smith House	circa 1914	Folk Victorian
Fuller House	circa 1928	Bungalow
Wilkerson Street Historic District	circa 1888-1950	Residential
Georgia Railroad Corridor & Contributing Properties	1874; 1883; 1890-1950	Linear; Commercial

PUBLIC INVOLVEMENT

Pursuant to 23 CFR 771.111, various public involvement efforts have been undertaken by the Department from the earliest stages of project planning. The public involvement efforts include committee meetings, advisory group meetings, and a public open house. The public open house was held on November 18, 2008. A total of 39 people were in attendance. In regards to cultural resources, the main concern of those in attendance was maintaining existing alignment and grades of the railroad bed and utilizing the existing structures crossing the North Oconee River and Trail Creek, specifically the Trail Creek Trestle. In response, it was confirmed that the existing vertical and horizontal alignment would be maintained in the design of the trail. It was

also explained that, at the time of this meeting, the existing structures were being evaluated for structural integrity for their proposed new use and that every effort would be made to keep the existing structures, but the cost to rehabilitate the structures, the cost of maintenance for this type of structure (railroad trestle), and the environmental impacts from this kind of structure are part of the evaluation. There was also a suggestion to place historic markers along the trail. The suggestion was confirmed as historic markers are planned for the trail where appropriate.

Table 2 presents an outline of the committee and advisory group meetings, including dates and meeting topics. Public comments concerning historic resources are provided in Appendix D. For a detailed listing of these meetings, please refer to the NEPA documentation.

Table 2. Summary of Project Design Meetings

Date	Group	Topic(s)
September 8, 2004	Committee	Land appraisals and acquisitions; distribution of funds
October 11, 2004	Committee	Project status and materials; funding recommendations
May 12, 2005	Committee	Project planning and land acquisition
May 18, 2005	Committee	Land appraisals and acquisition
December 7, 2005	Committee	CSX property appraisals and land acquisition
February 15, 2006	Committee	Funding status; property acquisition; creation of user groups
May 10, 2006	Committee	Review of funding and grants; land acquisition efforts
December 18, 2008	Committee	Bridge designs; funding; project updates
February 11, 2009	Advisory Group	Project update presentation
May 10, 2010	Committee	History of project; project status; trail and bridge design

DESCRIPTION OF HISTORIC PROPERTIES

Inglewood Avenue Mill House District

The Inglewood Avenue Mill House District is a residential district consisting of mill worker's housing. The district is located along Inglewood Avenue and a portion of the abandoned Georgia Railroad/CSX Transportation Rail Bed, between South Peters Street and Old Winterville Road (Figure 1). The district encompasses 18 properties, 16 of which are contributing.

Contributing properties within the district consist of single and multi-family dwellings, all of which are frame, Bungalow type mill housing, with the exception of 357 Little Oak Street, which is a Gable-Ell type. A portion of the Georgia Railroad Corridor is also included in the proposed district (Appendix B).

This property was evaluated for eligibility for listing in the National Register under Criteria A and C. The property possesses local and state levels of significance in the areas of community planning and development, and in the area of architecture. The dominance of mill employees living in the neighborhood and the area's association with the White City Manufacturing Company and the Bowen and Crews Company between 1913 and 1926 makes the proposed district eligible under Criterion A in the areas of community planning and development and social history. The proposed Inglewood Avenue Mill House District remains an unaltered and excellent example of early twentieth-century mill workers' housing in early twentieth-century Athens. The property also represents various house types identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings, and is representative of a significant trend in Georgia's architectural history making it eligible under Criterion C in the area of architecture.

Because the historic boundary is no longer intact and because there are no other significant or character defining features within the legal boundary that contribute to the architectural significance of the property, the eligible National Register boundary consists of a visual boundary. The eligible boundary, comprising approximately 12.15 acres, contains all National Register qualifying characteristics and features of the district (Appendix B).

Tweedell House

The Tweedell House is a Saddlebag house with no identified academic style located at 385 South Peter Street (Figure 1). The one-and-a-half-story frame building has a concrete slab foundation, lateral gable roof, and aluminum siding. The front elevation features dual front doors and a full front porch constructed of concrete block, a poured concrete floor, and a shed roof supported by decorative metal columns. The windows are three-over-one double hung sashes with six-over-six double hung sash windows in the roof gables. The rear porch of the house has been enclosed (Appendix B).

The Tweedell House was evaluated under Criterion C and appears to possess a local and state level of significance in the area of architecture as a representative house type in Georgia's architectural history that is identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. Although the rear porch has been enclosed and aluminum siding has been installed, the original design of the house, the overall plan, and roof shape of the house have been retained. The installation of aluminum siding merely obscures the original siding materials and can be removed. Finally, the rear porch has also been enclosed, however this change does not overwhelm the building and is not readily visible from the public right-of-way.

The eligible National Register boundary of the property corresponds to the legal property boundary and comprises approximately 0.23 acres. All significant and character defining features of the property are included within the legal boundary (Appendix B).

Smith House

The Smith House is a Folk Victorian style residence located at 375 South Peter Street (Figure 1). The frame building has vinyl siding, a multi-gable roof, and a full front porch comprised of a shed roof supported by turned wood posts. The symmetrical front façade features one-over-one double hung wooden sash windows framing the central front door. The two symmetrical front gables feature cornice returns, which continue into a cornice that is complete around the house eaves (including those on the rear addition). The house also has an interior brick chimney and stuccoed foundation. The rear porch of the house has been enclosed (Appendix B).

The Smith House was evaluated under Criterion C, and does appear to possess a local and state level of significance in the area of architecture. The property is representative of a house type in Georgia's architectural history as identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. The Smith House has maintained a fairly high degree of integrity in the area of design. Despite the rear addition, enclosed rear porch, and the installation of vinyl siding, the original design of the house is still clearly visible, as the rear alteration is not visible from the street. The house has retained significant features of the Folk Victorian style, such as the intersecting gable roof, porch configuration, and turned porch posts. Furthermore, the property has maintained its spatial relationship to the street, rail bed, and original lot layout of the neighborhood. Therefore, The Smith House is considered eligible for listing in the National Register under Criterion C for architecture.

The eligible National Register boundary of the property corresponds to the legal property boundary and comprises approximately .59 acres. All significant and character defining features of the property are included within the legal boundary (Appendix B).

Fuller House

The Fuller House is a Bungalow house type with Craftsman style influence located at 243 South Poplar Street (Figure 1). It is a frame building with aluminum siding and a brick pier foundation that has been in-filled with concrete block. The windows are six-over-six double hung wooden sash windows. It has a lateral gable roof with raised seam metal and exposed rafter tails in the eaves and knee braces in the gable ends. The shed-roofed front porch almost runs the width of the house. The building's basement/crawlspace is partially exposed (Appendix B).

The Fuller House was evaluated under Criterion C, and appears to possess a local and state level of significance in the area of architecture. The property is representative of the Bungalow house type and Craftsman style as identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. The property has maintained a high degree of architectural integrity, retaining its original design and most of its original materials. Therefore, the Fuller House is considered eligible for listing in the National Register under Criterion C for architecture.

The eligible National Register boundary of the property corresponds to the legal property boundary and comprises approximately 0.17 acres. All significant and character defining features of the property are included within the legal boundary (Appendix B).

Wilkerson Street Historic District

The Wilkerson Street Historic District is a residential district located within the city limits of Athens, approximately 0.5 miles southeast of downtown. The proposed district boundaries roughly follow East Broad Street to the north, the North Oconee River on the east, and the Georgia Railroad Corridor along the southeast (Figure 1). The district encompasses 17 properties, 15 of which are contributing. Contributing property building types include two Gabled Ells, one Central Hall, one Saddlebag, one New South Cottage, one Pyramid Cottage, one Side-Gabled Cottage, five Bungalows, three American Small House types, and one commercial building. There are four empty lots, all of which have concrete stairs or driveways indicating where the houses once stood (Appendix B).

The Wilkerson Street Historic District was evaluated under Criteria A and C, possessing local and state significance in the areas of community planning and development, social history, and design. Under Criterion A, the district is representative of a working class neighborhood that developed between the late eighteenth and mid-twentieth century. The various house types are typical of workers' housing in Athens during this period of commercial and industrial development. As a whole, the proposed district has maintained integrity under Criterion C in the area of architectural design. All of the commercial and residential building types found within the proposed Wilkerson Street Historic District are identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings and represent Georgia's architectural history during the late nineteenth and early half of the twentieth century. The combination of house types and age physically conveys the heritage of Athens' working class through the evolution of the built environment.

Because the historic boundary is no longer intact and because there are no other significant or character defining features within the legal boundary that contribute to the architectural significance of the property, the eligible National Register boundary consists of a visual boundary. The eligible boundary, comprising approximately 10 acres, contains all National Register qualifying characteristics and features of the district (Appendix B).

Georgia Railroad Corridor and Contributing Properties

The Georgia Railroad Corridor and Contributing Properties is a linear resource that extends through the project area from the intersection with Old Winterville Road northwest to East Broad Street. The resource extends from the southwest side of Little Oak Street and Old Winterville Road, north to East Broad Street. It includes buildings bounded by East Broad Street, Oconee Street, Wilkerson Street, and the Central of Georgia Railroad Corridor, as well as the individual property located at 805 East Broad Street (Figure 1).

The rail spur primarily consists of a raised bed and is lacking such physical elements as gravel, asphalt, concrete, rails, or ballasts. The trestle remnant on the Georgia Railroad corridor at South Poplar and South Peter streets consists of a timber structure, constructed of creosote-treated pilings resting on concrete footings. Three vertical piles flanked by diagonal piles create the pile bents, each supporting a pile cap. The structure is in a fairly advanced state of disrepair in part due to partial demolition that occurred in 2000. Vestiges of the structure on the southeast side of

South Poplar Street include two vertical timber piles and a timber retaining wall. A portion of a timber retaining wall and four concrete footings are also found along South Peter Street. The remnant of trestle bridge currently extends approximately 350 feet. A total of six, tall granite bents and six concrete footings are all that remain of the former North Oconee Bridge on the Georgia Railroad corridor. Three bents each are located on both the north and south sides of the river and are partially covered in vegetation. The six concrete footings are adjacent to Wilkerson Road, three on each side. Contributing properties include a group of both masonry and frame warehouse buildings that line both sides of the Georgia Railroad corridor, East Broad Street, Oconee Street, and Wilkerson Street, and the railroad corridor roughly bound the cluster of contributing properties.

The Georgia Railroad Corridor and Contributing Properties was evaluated under Criteria A and appears to possess a state level of significance in the area of transportation, commerce, economics, industry, and community planning and development. The Georgia Railroad served as a major transportation route running from Augusta to Atlanta and historically has been an integral part of a larger rail network system that connected major shipping and transport hubs in the east to other major cities located in the interior southeastern United States. The contributing warehouse properties located at the north end of the corridor are associated with the industrial and commercial development of Athens and the city's dependence on cotton and textile manufacturing. The concentration of commercial and industrial development along the rail corridor underscores the importance of the railroad access for shipping and transport during the late nineteenth and early twentieth centuries. Therefore, the Georgia Railroad Corridor and Contributing Properties are considered eligible under Criterion A.

The Georgia Railroad Corridor and Contributing Properties was evaluated under Criterion C and appears to possess a state level of significance in the area of engineering and a local level of significance in the area of architecture. Despite the overgrowth of vegetation and loss of most character-defining features, the raised railroad bed is still evident on the property. Remnants of the corridor's former bridge and trestle are also present. The contributing frame and brick commercial buildings are examples of warehouse design and construction dating from the late nineteenth and early twentieth centuries of Georgia's architectural history. Therefore, the Georgia Railroad Corridor and Contributing Properties are eligible under Criterion C.

The proposed National Register boundary for the Georgia Railroad Corridor and Contributing Properties corresponds to the existing railroad right-of-way and includes all character-defining features including the wooden rail trestle, masonry bridge bents, and the contiguous cluster of commercial warehouses bounded by East Broad Street, Oconee Street, Wilkerson Street and the Central of Georgia Railroad Corridor, as well as the individual property located at 805 East Broad Street. The proposed boundary contains approximately 29.25 acres and runs roughly 1.13 miles from the intersection of Little Oak Street and Winterville Road, north to the northeast corner of Parcel Number 171B2 A006A, and roughly 0.15 miles east to west, from the eastern edge of Parcel Number 171B2 A006A west to Oconee Street (Appendix B).

ASSESSMENT OF EFFECTS

Inglewood Avenue Mill House District

A finding of No Adverse Effect is anticipated for the Inglewood Avenue Mill House District. In the area of the resource, project implementation would consist of the use of the abandoned Georgia Railroad bed, a contributing resource, to create a paved pedestrian/bicycle trail (Figure 2). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. A portion of the trail would have a handrail along one side. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height.

Physical destruction of or damage to all or part of the district would not occur. The railroad corridor, a contributing element to the district, would be paved in order to create the pedestrian/bicycle trail. While this action would affect the district, it would not be an adverse effect. The trail would follow the existing grade and alignment and the rails and ties were removed after the spur was abandoned in 1998. The proposed project would not have any effect on the residences within the district, as the railroad corridor is at least 80 feet from the residences, separated by trees.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Inglewood Avenue Mill House District that would alter the character of the continued residential and transportation uses of the district. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The proposed bicycle/pedestrian trail would follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed as well as its relationship to the neighborhood and the overall setting and character of the area. Since the 22-foot wide trail is within the cleared railroad corridor, no trees would be removed in order to construct the trail. Project implementation would not remove any significant historic landscapes, buildings, or features of the resource.

Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The railroad corridor would still be an element of the residential district. The residences of the district are shielded from the railroad corridor by vegetation and distance. The distance from the district to the railroad corridor would not change. The visual perception from the district would not change. The visual character of the area surrounding the district has been compromised by modern commercial and residential development.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There

Figure 2
Proposed Improvements in the Area of the Inglewood Avenue Millhouse District
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Aerial Photography

would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Inglewood Avenue Mill House District. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is not anticipated to adversely affect the Inglewood Avenue Mill House District. No change in traffic patterns would result from project implementation. No additional access to the existing roadway would be provided and no existing access to the roadway would be removed. The proposed project would not increase the capacity of the existing roadway. The proposed project would create a pedestrian/bicycle facility connecting two important transportation links, preserve and enhance the railroad corridor, and provide the community with a recreation facility.

Tweedell House

A finding of No Effect is anticipated for the Tweedell House. In the area of the resource, project implementation would consist of the use of the abandoned Georgia Railroad bed to create a pedestrian/bicycle trail (Figure 3). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. A portion of the trail would have a handrail along one side. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height.

Physical destruction of or damage to all or part of the property would not occur as no ground disturbing activity would occur within the eligible boundary.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Tweedell House that would alter the character of the continued residential use of the property. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The property is located approximately 160 feet southwest of the project area, separated by a residential property and trees. The proposed bicycle/pedestrian trail would follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed, as well as its relationship to the neighborhood and the overall setting and character of the area. Since the 22-foot wide trail is within the cleared railroad corridor, no trees would be removed in order to construct the trail. Project implementation would not remove any significant historic landscapes, buildings, or features of the resource.

Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The railroad corridor always has been and would continue to be an element of the visual character of the property.

Figure 3
Proposed Improvements in the Area of the Tweedell House
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Bing Aerial Photography

However, the property is shielded from the railroad corridor by vegetation and a residential property. Therefore, enhancements introduced along the trail, such as the handrails, would be obscured from the property. The distance from the property to the railroad bed would not change. The visual perception from the property would not change. The visual character of the area surrounding the resource has been compromised already by modern commercial and residential development.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Tweedell House. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is not anticipated to affect the Tweedell House. No change in traffic patterns would result from project implementation. No additional access to the existing roadway would be provided and no existing access to the roadway would be removed. The proposed project would not increase the capacity of the existing roadway. The proposed project would create a pedestrian/bicycle facility connecting two important transportation links, preserve and enhance the railroad corridor, and provide the community with a recreation facility.

Smith House

A finding of No Effect is anticipated for the Smith House. In the area of the resource, project implementation would consist of the use of the abandoned Georgia Railroad bed to create a pedestrian/bicycle trail (Figure 4). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. A portion of the trail would have a handrail along one side. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height.

Physical destruction of or damage to all or part of the property would not occur as no ground disturbing activity would occur within the eligible boundary.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Smith House that would alter the character of the continued residential use of the property. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The property is located approximately 40 feet southwest of the project area, separated by trees. The proposed bicycle/pedestrian trail would follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed as well as its relationship to the neighborhood and the

Figure 3
Proposed Improvements in the Area of the Tweedell House
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Bing Aerial Photography

overall setting and character of the area. Since the 22-foot wide trail is within the cleared railroad corridor, no trees would be removed in order to construct the trail. Project implementation would not remove any significant historic landscapes, buildings, or features of the resource.

Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The railroad corridor always has been and would continue to be an element of the visual character of the property. However, the property is shielded from the railroad corridor by vegetation. Therefore, enhancements introduced along the trail, such as the handrails, would be obscured from the property. The distance from the property to the railroad bed would not change. The visual perception from the property would not change. The visual character of the area surrounding the resource has been compromised already by modern commercial and residential development.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Smith House. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is not anticipated to indirectly affect the Smith House. No change in traffic patterns would result from project implementation. No additional access to the existing roadway would be provided and no existing access to the roadway would be removed. The proposed project would not increase the capacity of the existing roadway. The proposed project would create a pedestrian/bicycle facility connecting two important transportation links, preserve and enhance the railroad corridor, and provide the community with a recreation facility.

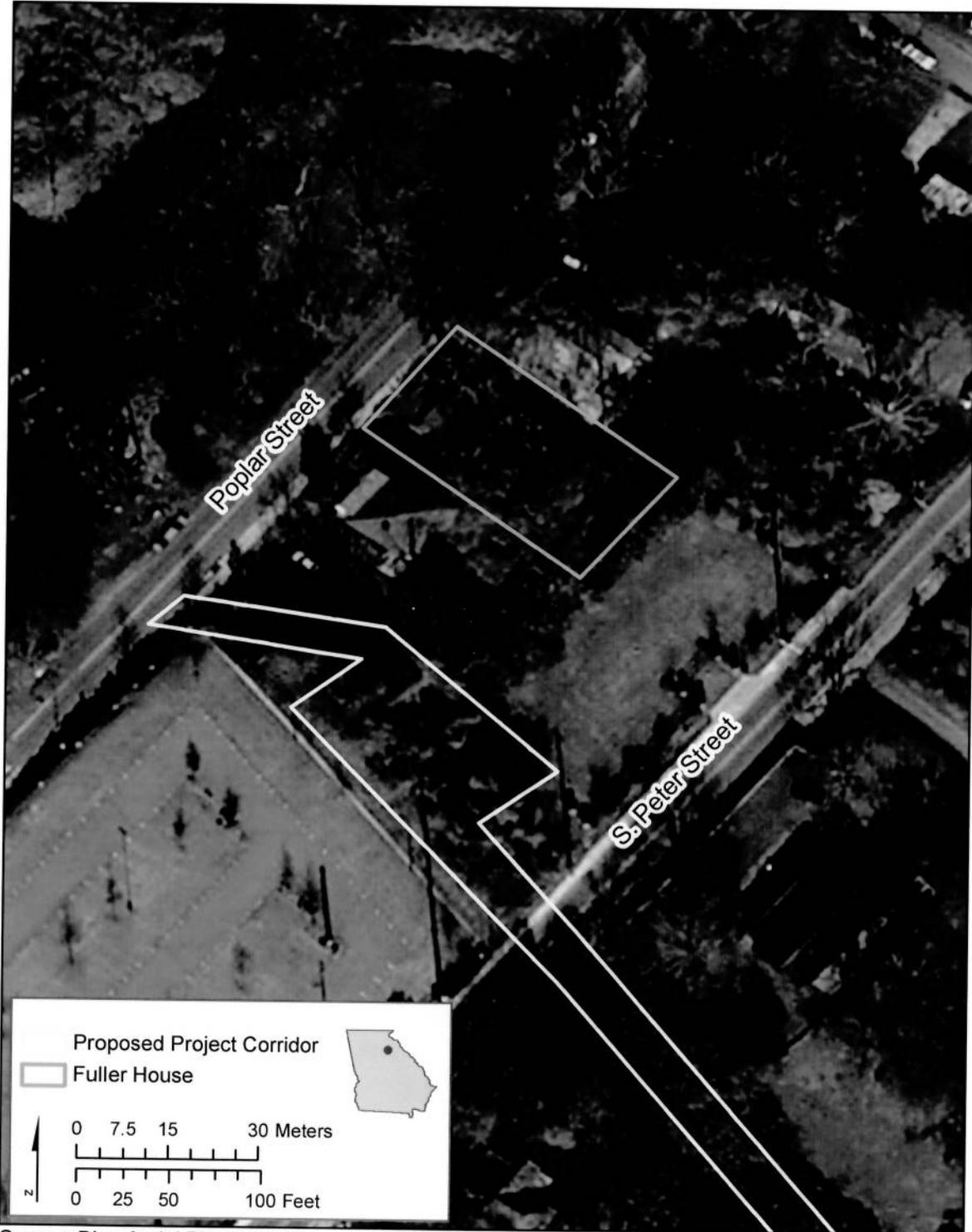
Fuller House

A finding of No Effect is anticipated for the Fuller House. In the area of the resource, project implementation would consist of the use of the abandoned Georgia Railroad bed to create a pedestrian/bicycle trail (Figure 5). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. A portion of the trail would have a handrail along one side. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height. An ADA ramp connects the trail to Poplar Street, spanning the difference in grade through a series of switchbacks.

Physical destruction of or damage to all or part of the property would not occur as no ground disturbing activity would occur within the eligible boundary.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Fuller House that would alter the character of

Figure 5
Proposed Improvements in the Area of the Fuller House
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Bing Aerial Photography

the continued residential use of the property. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The property is located approximately 90 feet northeast of the project area, separated by a residence and trees. The proposed bicycle/pedestrian trail would follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed, as well as its relationship to the neighborhood and the overall setting and character of the area. Since the 22-foot wide trail is within the cleared railroad corridor, no trees would be removed in order to construct the trail. Project implementation would not remove any significant historic landscapes, buildings, or features of the resource.

Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The railroad corridor always has been and would continue to be an element of the visual character of the property. However, the property is shielded from the railroad corridor by vegetation and a residential property. Therefore, enhancements introduced along the trail, such as the handrails, would be obscured from the property. The distance from the property to the railroad bed would not change. The visual perception from the property would not change. The visual character of the area surrounding the resource has been compromised already by modern commercial and residential development.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Fuller House. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is not anticipated to indirectly affect the Smith House. No change in traffic patterns would result from project implementation. No additional access to the existing roadway would be provided and no existing access to the roadway would be removed. The proposed project would not increase the capacity of the existing roadway. The proposed project would create a pedestrian/bicycle facility connecting two important transportation links, preserve and enhance the railroad corridor, and provide the community with a recreation facility.

Wilkerson Street Historic District

A finding of No Effect is anticipated for the Wilkerson Street Historic District. In the area of the resource, project implementation would consist of the use of the abandoned Georgia Railroad bed to create a pedestrian/bicycle trail (Figure 6). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. A portion of the trail would

have a handrail along one side. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height.

Physical destruction of or damage to all or part of the property would not occur as no ground disturbing activity would occur within the eligible boundary.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Wilkerson Street Historic District that would alter the character of the continued residential use of the district. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The property is located directly northeast of the project area, separated by trees. The proposed bicycle/pedestrian trail would follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed, as well as its relationship to the remainder of the district and the overall setting and character of the district. Since the 22-foot wide trail is within the cleared railroad corridor, no trees would be removed in order to construct the trail. Project implementation would not remove any significant historic landscapes, buildings, or features of the resource.

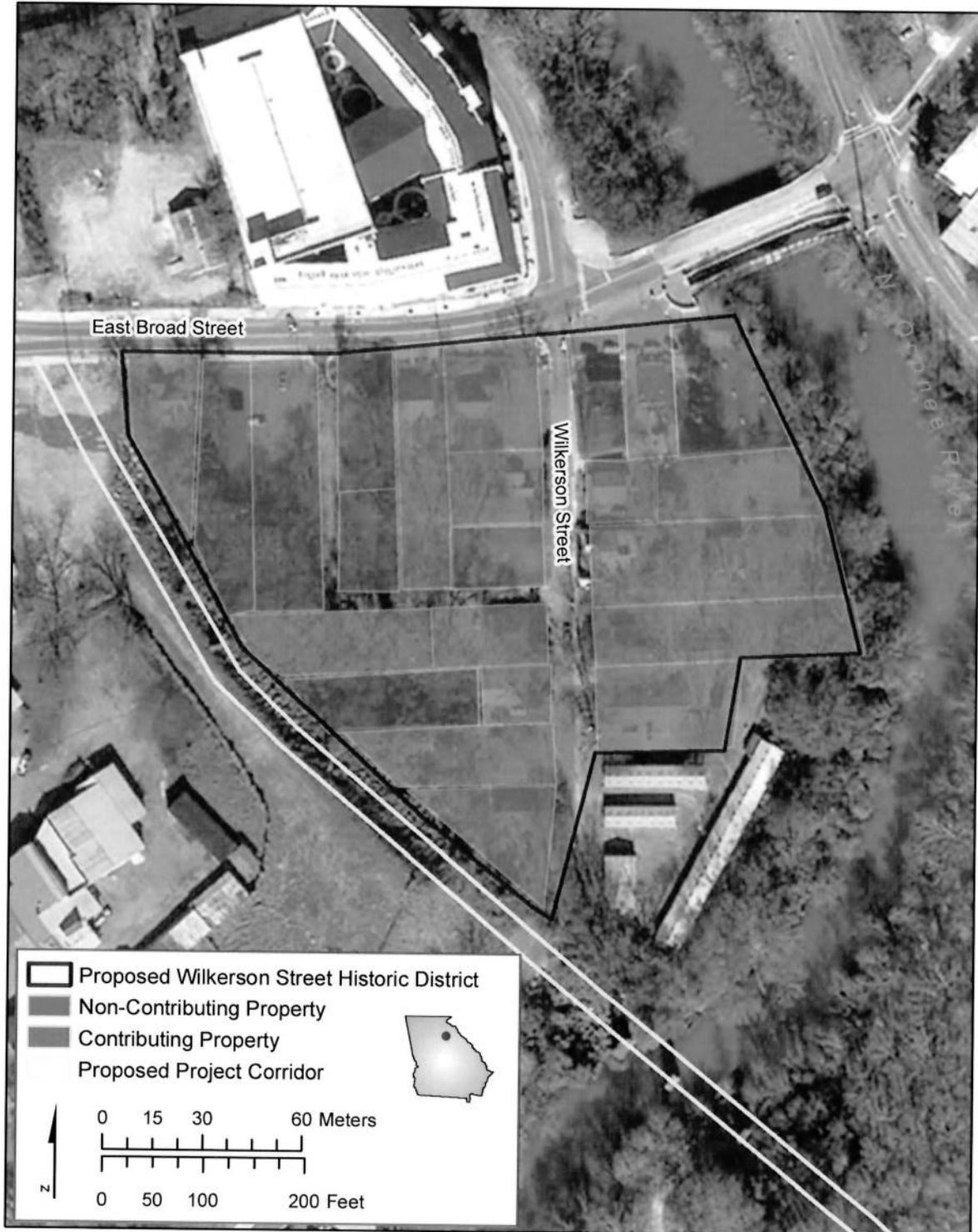
Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The railroad corridor always has been and would continue to be an element of the visual character of the district. However, the property is shielded from the railroad corridor by vegetation. Therefore, enhancements introduced along the trail, such as the handrails, would be obscured from the property. The distance from the property to the railroad corridor would not change. The visual perception from the property would not change. The visual character of the area surrounding the resource has been compromised already by modern commercial and residential development.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Wilkerson Street Historic District. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is not anticipated to indirectly affect the Wilkerson Street Historic District. No change in traffic patterns would result from project implementation. No additional access to the existing roadway would be provided and no existing access to the roadway would be removed. The proposed project would not increase the capacity of the existing roadway. The proposed project would create a pedestrian/bicycle facility connecting two important transportation links, preserve and enhance the railroad corridor, and provide the community with a recreation facility.

Figure 6
Proposed Improvements in the Area of the Wilkerson Street Historic District
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Georgia Railroad Corridor and Contributing Properties

A finding of Adverse Effect is anticipated for the Georgia Railroad Corridor and Contributing Properties. In the area of the district, project implementation would include the use of the abandoned Georgia Railroad bed along with existing walkways to create a pedestrian/bicycle trail (Figure 7). The paved trail would be approximately 14 feet wide with a four-foot clear area along each side, for a total width of 22 feet. The trail would be paved with a six-inch aggregate base and two inches of asphalt. Portions of the trail would have a handrail along one or both sides. The handrail, constructed of galvanized posts and five, parallel wooden railings, would measure approximately four-and-a-half feet in height. The proposed pedestrian/bicycle trail would follow the existing grade and alignment of the rail bed from Old Winterville Road, recreating the historic alignment of the railroad over Peter Street with a pedestrian foot bridge, to the east side of Poplar Street. The proposed pedestrian/bicycle trail would connect with the existing sidewalk system and cross Poplar Street before joining with the existing walkway system into Dudley Park. The path of travel would follow the existing walkway system through Dudley Park until a new section of trail is constructed to re-traverse from grade back to the abandoned railroad bed on the west side of Trail Creek. The trail would continue along the abandoned railroad bed west of Trail Creek for a short distance before the North Oconee River crossing. The abandoned North Oconee River crossing contains existing piers of a bridge over the North Oconee River and Wilkerson Street. The project would include the design and construction of a pedestrian bridge utilizing the existing piers to span the North Oconee River and Wilkerson Street. The trail then returns to grade along the abandoned railroad bed west of the North Oconee River and Wilkerson Street until terminating at grade at East Broad Street, the end of the project.

Physical destruction of or damage to all or part of the property would not occur. The railroad corridor, a contributing element to the district, would be paved in order to create the pedestrian/bicycle trail. While this action would affect the district, it would not be an adverse effect. The trail would primarily follow the existing grade and alignment; only leaving the railroad corridor at Poplar Street in order to bypass the Trail Creek Trestle, following existing and proposed paths through Dudley Park before rejoining the railroad corridor on the west side of Trail Creek. Additionally, the ballasts, rails, and ties were removed after the spur was abandoned in 1998. The proposed project would not have any effect on the buildings within the district, as the railroad corridor is at least 80 feet from the buildings, separated by trees.

Project implementation would not result in a change in the character of the property's use. There are no direct or indirect effects anticipated to the Georgia Railroad Corridor and Contributing Properties that would alter the character of the continued commercial and transportation uses of the district. The Trail Creek Trestle would be avoided and not altered. Therefore, project implementation would not result in a change in the character of the property's use.

Project implementation would not result in a change in the character of the property's physical features within the property's setting that contribute to its historic significance. The proposed pedestrian/bicycle trail would primarily follow the existing grade and alignment of the rail bed, preserving the historic character of the rail bed and associated features, as well as its relationship to contributing properties. The Trail Creek Trestle would be avoided and would not be altered

Figure 7
Proposed Improvements in the Area of the
Georgia Railroad Corridor and Contributing Properties
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Aerial Photography

physically or visually by the trail or the overlook. The overall setting and character of the property would also be maintained. Any visual change to the property would be negligible, as the ballasts, rails, and ties are no longer present along the rail bed and therefore would not be removed during implementation of the proposed project. In addition, the proposed pedestrian/bicycle trail would restore the historic alignment of the Georgia Railroad over Peter Street and the North Oconee River.

Project implementation would not result in the introduction of visual elements that diminish the integrity of the property's significant historic characteristics or features. The existing grade and alignment always has been and would continue to be an element of the visual character of the property. The railing and interpretive panels installed on the overlook would be similar in scale and materials as existing railings and/or fencing. In addition, the property would continue to be used as a transportation facility. The visual perception from the property would not change.

Project implementation would not result in the introduction of atmospheric elements that diminish the integrity of the property's significant historic characteristics or features. There would be no atmospheric effect to this property as a result of project implementation. The project is consistent with the State Implementation Plan for air quality in the region.

Project implementation would not audibly affect the Georgia Railroad Corridor and Contributing Properties. The bicycle/pedestrian path is not considered a noise generator. Therefore, a noise study is not required and no noise impacts would occur due to the implementation of this project.

Project implementation is anticipated to cause an indirect adverse effect to the Georgia Railroad Corridor and Contributing Properties due to the planned benign neglect of the Trail Creek Trestle. Under 36 CFR Part 800.5 (a)(2)(vi) of Section 106 of the National Historic Preservation Act, an adverse effect occurs when the undertaking results in the "neglect of a property that causes its deterioration, except where such neglect and deterioration are recognized are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization." The financial costs involved to maintain the trestle are prohibitive and not within the Athens-Clarke County government's budget. The trestle is constructed with creosote-soaked timbers, which pose a potential health hazard that the county does not want to be liable for. Therefore, the trestle would be neglected, which would cause an indirect adverse effect on the Georgia Railroad Corridor and Contributing Properties.

SUMMARY

Implementation of the proposed project would result in a finding of No Effect for the Tweedell House, the Smith House, the Fuller House, and the Wilkerson Street Historic District.

Implementation of the proposed project would result in a finding of No Adverse Effect for the Inglewood Avenue Mill House District.

Implementation of the proposed project would result in an Adverse Effect for the Georgia Railroad Corridor and Contributing Properties. See a detailed summary in Appendix C.

ALTERNATIVES TO AVOID ADVERSE EFFECT

Three additional proposed design alternatives were discussed prior to the selection of the current preferred trail route (Alternative B). The committee examined the alternatives and their anticipated effects were discussed.

Under the No Build Alternative, no pedestrian/bicycle trail would be constructed. This alternative was eliminated due to the need for this the facility to provide a needed link between the two transportation facilities as well as community support for this type of facility. In addition, the No Build Alternative would still create an adverse effect as the trestle would not be rehabilitated or maintained.

Alternative A moved the proposed trail off the historic railroad bed and along US 78, a heavily trafficked highway. This alternative did not meet the need and purpose of the project.

Alternative C closely followed the historic alignment of the railroad but would construct a new pedestrian bridge parallel to the trestle, which would be left in place. Tie-ins to Poplar Street and Dudley Park would also be included. While this alignment would avoid the trestle, it still causes an adverse effect. The new bridge would visually impact the trestle and the trestle would not be rehabilitated or maintained. Additionally, this alternate was not selected as the cost was significantly higher than the preferred trail route (Alternative B) and, therefore, is not the most prudent and feasible option.

Alternative D retained the historic alignment of the railroad by removing the trestle and replacing it with a new bridge over Poplar Street. Tie-ins to Poplar Street and Dudley Park would also be included. The removal of the trestle would result in an adverse effect.

PLANNING TO MINIMIZE HARM AND PROPOSED MITIGATION

Planning to minimize harm was taken into consideration to the extent possible during project development. The original project alignment was altered to avoid directly impacting the Trail Creek Trestle. Instead of altering the historic fabric of the trestle for reuse, or demolishing the trestle to facilitate construction of a new bridge that would carry bicycle and pedestrian traffic across Trail Cree, the proposed project alignment would now drop from the existing rail bed grade to street grade on the south side of Poplar Street using a ramp with several switchback design to meet ADA and other applicable standards. The proposed alignment would then cross Poplar Street and join the existing rail bed grade on the south side of the North Oconee River.

The following mitigation measures are proposed for discussion at consultation between FHWA and the SHPO:

- 1) Since the Trail Creek Trestle will not be maintained or rehabilitated, historical and photographic documentation of the Trestle will be completed before it deteriorates further. This documentation will consist of the development of a brief architectural description and

historical narrative of the trestle. The Trail Creek Trestle will be photographed with high resolution digital photography per standards previously agreed upon between GDOT and SHPO in the Permanent Archival Record Manual. The photography and accompanying documentation will be submitted to the SHPO for acceptance and retention.

- 1) Interpretive signage providing historical data about the Trail Creek Trestle will be produced and installed in conjunction with project implementation. This sign will be of comparable size, style, and materials as the existing interpretive panels within Dudley Park.

APPENDICES

APPENDIX A
NOTIFICATION
AND
EARLY CONSULTATION CORRESPONDENCE



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NOTIFICATION

**Initiation of Section 106 Process for
GDOT Project CSHPP-0007-00(561), Athens-Clarke County
P.I. # 0007561
July 25, 2008**

The Georgia Department of Transportation is in the beginning stages of project development for this proposed transportation project. In compliance with Section 106 of the National Historic Preservation Act, the Department has determined that because of the nature and the scope of this undertaking, the proposed project has the potential to cause effects to historic properties if any such properties exist in the project area. The Department is attempting to identify historic properties already listed in the National Register of Historic Places and any properties not already listed that would be considered eligible for listing that are located within the geographic area of potential effects (APE) of the proposed project.

The proposed project would consist of the renovation of approximately 1 mile of an abandoned railroad bed as a trail. The entire project is in Athens-Clarke County. The abandoned railroad bed extends from the intersection with Old Winterville Road and ends on East Broad Street. The trail will begin at grade from Old Winterville Road and follow along the abandoned railroad bed to Peter Street. A single span prefabricated pedestrian bridge structure will be used to cross Peter Street and then the path follows a short distance at grade to another single span prefabricated pedestrian bridge over Poplar Street. The existing structure crossing Trail Creek is a wooden trestle better known as the Murmur Trestle. The trestle once crossed Poplar Street but is no longer complete. The trestle will be inspected for structural soundness and ability to support the trail. The trail then returns to grade for a short distance to the North Oconee River crossing. This abandoned section of the railroad contains existing piers of a bridge over the North Oconee River and Wilkerson Street. The project will include the design and construction of a pedestrian bridge utilizing the existing piers to span the North Oconee River and Wilkerson Street. The trail will then come back on grade for a short section before the end of the project at East Broad Street. The trail will be contained in 14 feet of right-of-way centered on the existing rail within the existing CSX Railroad right-of-way.

Because of the nature and scope of the undertaking, the APE is limited to the existing railroad right-of-way and viewshed of the proposed project, within which all construction and ground disturbing activity would be confined (see attached location map). No potential for indirect effects outside of the existing right-of-way and viewshed of the proposed project exists.

Section 106 of the National Historic Preservation Act requires the Federal Highway Administration and the Georgia Department of Transportation, in consultation with the Georgia State Historic Preservation Officer, to identify potential consulting parties and to invite them to participate in the Section 106 process. This Notification letter is one of several methods the Department uses to encourage public participation in this process and it serves as your invitation to participate as a consulting party in the Section 106 process for this project.

A written request to become a consulting party for cultural resources for this project should be directed to:

Theresa M. Hamby
New South Associates, Inc.
6150 East Ponce de Leon Avenue
Stone Mountain, Georgia 30083

Responses would be appreciated within thirty (30) days of receipt of this Notification letter. Please refer to the project identification number P.I. 0007651 in your response. The potential consulting parties identified and invited to participate in the Section 106 process for this project are the Northeast Georgia Regional Development Center, Georgia SHPO, the Clarke County Commission, the Athens Historical Society, the Athens-Clarke County Library, and the Athens-Clarke Heritage Foundation. If you are aware of other organizations or individuals interested in cultural resources in the project area not already identified, please forward their names to the Department.

Also, on behalf of the Federal Highway Administration Georgia Division (FHWA), in keeping with a government-to-government relationship and in compliance with 36CFR800, the following tribal governments are invited to participate in the Section 106 process for this project: Eastern Band of Cherokee Indians of North Carolina, Muscogee (Creek) Nation, Muscogee (Creek) National Council, Poarch Band of Creek Indians, Seminole Tribe of Florida, Thlopthlocco Tribal Town, and the United Keetoowah Band of Indians. Responses to this Notification regarding tribal concerns should be addressed to the attention of Mr. Eric Duff, the Department's Native American liaison, at the above address.

Existing information on previously identified historic properties has been checked to determine if any are located within the APE of this undertaking. This review of existing information revealed that the northwestern terminus of the corridor appears to be adjacent to the southern boundary of the Athens Warehouse District, which is described as roughly bounded by Hancock and Thomas streets and the railroad tracks. The district was listed on the National Register in 1988. There are no National Historic Landmarks, and no bridges determined eligible for inclusion in the National Register in the updated Georgia Historic Bridge Survey (GHBS) in the APE.

The trail corridor is located in the vicinity of three areas noted by Amber Eskew, Athens-Clarke County Preservation Planner, as potential local districts. These areas include Ingersall, East Athens, and Potterytown. Also, the portion of the Poplar Street Trestle Bridge over Trail Creek

that is still extant, known as the Murmur Bridge, is a local landmark of known interest to Athens-Clarke County.

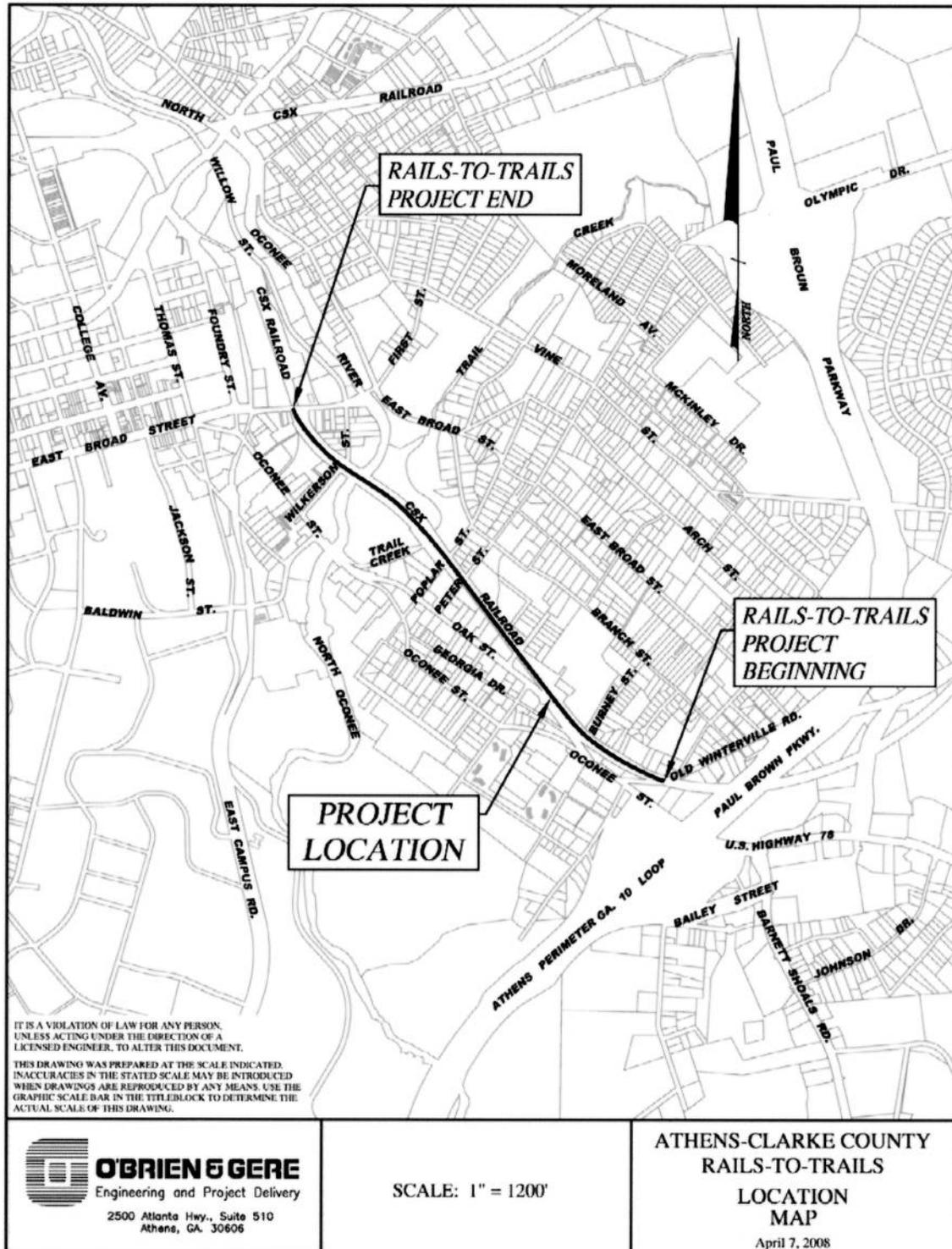
One previously recorded resource, the Cook & Brother Armory, Chicopee Mills (ID 4594) is located just northeast of the APE at the western end of the trail corridor. No properties 50 years old or older were identified within the proposed project's APE in the 1988 Department of Natural Resources (DNR) Clarke County, Athens survey.

Even though no properties 50 years old or older were identified within the APE in the 1988 DNR survey, the survey information is outdated. Thus the proposed project will be field surveyed for both historic properties and archaeological sites and the Criteria of Eligibility will be applied to any identified properties in consultation with the Georgia SHPO and other consulting parties to determine if any of those properties are eligible for inclusion in the National Register of Historic Places.

Consulting parties are also invited to provide information concerning any historic or archaeological properties already listed in the National Register or that could be eligible for listing in the National Register that are not identified in this Notification letter. In accordance with Section 106 of the National Historic Preservation Act, the Department will assess project effects to any identified historic properties as preliminary project plans become available, endeavor to minimize harm to all identified historic properties and produce an Assessment of Effects report. This document will be provided to all consulting parties for comment when completed.

Individuals and organizations that do not wish to become a consulting party, but would still like to comment on the proposed project will also have that opportunity throughout the plan development process. Historic resource concerns can be addressed to Christina Olson (770-498-4155 ext. 124 or colson@newsouthassoc.com); archaeological resource concerns, including cemetery and other human burials, can be addressed to Theresa M. Hamby (770-498-4155 ext. 122 or thamby@newsouthassoc.com). Questions concerning general design or location issues may be addressed to Maureen O'Brien (706-548-4881 or OBrienMK@obg.com).

Figure 1 – Project Location
 Project Number: CSHP-0007-00(561)
 PI Number: 0007561



Georgia Department of Natural Resources

Noel Holcomb, Commissioner

Historic Preservation Division

W. Ray Luce, Division Director and Deputy State Historic Preservation Officer
34 Peachtree Street, NW, Suite 1600, Atlanta, Georgia 30303
Telephone (404) 656-2840 Fax (404) 657-1040 <http://www.gashpo.org>

MEMORANDUM

TO: Glenn S. Bowman
State Environment/Location Engineer
Office of Environment and Location
Georgia Department of Transportation
Attention: Sandy Lawrence

FROM: Amanda Schraner *AS*
Transportation Projects Coordinator
Historic Preservation Division

RE: Receipt of Early Coordination Information

Project Title: P.I. #0007561; CSHPP-0007-00(561)
One Mile of Rail to Trail from Old Winterville Road to East Broad Street, Athens

Project Number: HP-080728-001

County: Clarke

DATE: August 5, 2008

The Historic Preservation Division has received the early coordination information required by Section 106 of the National Historic Preservation Act and the Georgia Environmental Policy Act (GEPA). Thank you for submitting this information, and we look forward to working with you in the future as this project progresses.

AS:mav

cc: Theresa Hamby, New South Associates, Inc.



Georgia Department
of Natural Resources

Department of Natural Resources
Historic Preservation Division
34 Peachtree Street, NW
Suite 1600
Atlanta, GA 30303

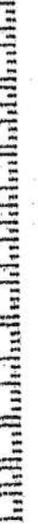


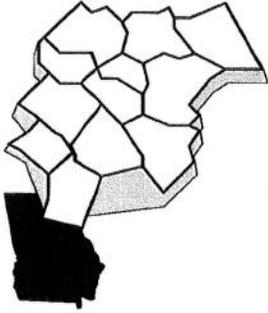
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Theresa Hamby
New South Associates
6150 East Ponce de Leon Ave.
Stone Mountain, GA 30083

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NORTHEAST GEORGIA REGIONAL DEVELOPMENT CENTER
305 Research Drive Athens, Georgia 30605-2795
(706) 369-5650 • Fax (706) 369-5792

James R. Dove
Executive Director

August 18, 2008

Theresa M. Hamby
New South Associates, Inc.
6150 East Ponce de Leon Avenue
Stone Mountain, GA 30083

Re: P.I. 7561

Dear Ms. Hamby:

The above-referenced project was reviewed, as requested, to provide comments in assisting the Georgia Department of Transportation in identifying historic properties listed in, or eligible for, the National Register of Historic Places.

Based on available information in our offices, no properties listed in the National Register of Historic Places were identified in the project's area.

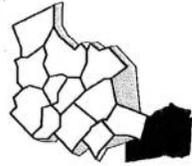
Information regarding archaeological resources is unavailable to us in our offices; we are unable to comment on their known existence.

If you have further questions regarding this information or if we can assist further, please contact Lee Carmon, Planning Director, at lcarmon@negplanning.org.

Sincerely,

James R. Dove
Executive Director

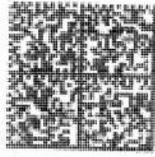
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NORTHEAST GEORGIA REGIONAL DEVELOPMENT CENTER
305 Research Drive Athens, Georgia 30605-2795



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Theresa M. Hamby
New South Associates, Inc.
6150 East Ponce de Leon Avenue
Stone Mountain, GA 30083

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Gena L. Evans, Ph.D., Commissioner



DEPARTMENT OF TRANSPORTATION

One Georgia Center, 600 West Peachtree Street, NW
Atlanta, Georgia 30308
Telephone: (404) 631-1000

NOTIFICATION

Initiation of Section 106 Process for GDOT Project CSHPP-0007-00(561), Athens-Clarke County

P.I. # 0007561

March 4, 2009

The Georgia Department of Transportation is in the beginning stages of project development for this proposed transportation project. In compliance with Section 106 of the National Historic Preservation Act, the Department has determined that because of the nature and the scope of this undertaking, the proposed project has the potential to cause effects to historic properties if any such properties exist in the project area. The Department is attempting to identify historic properties already listed in the National Register of Historic Places and any properties not already listed that would be considered eligible for listing that are located within the geographic area of potential effects (APE) of the proposed project.

The proposed project would consist of the renovation of approximately 1 mile of an abandoned railroad bed as a trail. The entire project is in Athens-Clarke County. The abandoned railroad bed extends from the intersection with Old Winterville Road and ends on East Broad Street. The trail will begin at grade from Old Winterville Road and follow along the abandoned railroad bed to Peter Street. A single span prefabricated pedestrian bridge structure will be used to cross Peter Street and then the path follows a short distance at grade to another single span prefabricated pedestrian bridge over Poplar Street. The existing structure crossing Trail Creek is a wooden trestle better known as the Murmur Trestle. The trestle once crossed Poplar Street but is no longer complete. The trestle will be inspected for structural soundness and ability to support the trail. The trail then returns to grade for a short distance to the North Oconee River crossing. This abandoned section of the railroad contains existing piers of a bridge over the North Oconee River and Wilkerson Street. The project will include the design and construction of a pedestrian bridge utilizing the existing piers to span the North Oconee River and Wilkerson Street. The trail will then come back on grade for a short section before the end of the project at East Broad Street. The trail will be contained in 14 feet of right-of-way centered on the existing rail within the existing CSX Railroad right-of-way.

Because of the nature and scope of the undertaking, the APE is limited to the existing railroad right-of-way and viewshed of the proposed project, within which all construction and ground disturbing activity would be confined (see attached location map). No potential for indirect effects outside of the existing right-of-way and viewshed of the proposed project exists.

Section 106 of the National Historic Preservation Act requires the Federal Highway Administration and the Georgia Department of Transportation, in consultation with the Georgia State Historic Preservation Officer, to identify potential consulting parties and to invite them to participate in the Section 106 process. This Notification letter is one of several methods the Department uses to encourage public participation in this process and it serves as your invitation to participate as a consulting party in the Section 106 process for this project.

A written request to become a consulting party for cultural resources for this project should be directed to:

Theresa M. Hamby
New South Associates, Inc.
6150 East Ponce de Leon Avenue
Stone Mountain, Georgia 30083

Responses would be appreciated within thirty (30) days of receipt of this Notification letter. Please refer to the project identification number P.I. 0007651 in your response. The potential consulting parties identified and invited to participate in the Section 106 process for this project are the Northeast Georgia Regional Development Center, Georgia SHPO, the Clarke County Commission, the Athens Historical Society, the Athens-Clarke County Library, the Athens-Clarke Heritage Foundation, and the Athens-Clarke County Historic Preservation Commission. If you are aware of other organizations or individuals interested in cultural resources in the project area not already identified, please forward their names to the Department.

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RECEIVED
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DEPARTMENT OF TRANSPORTATION

STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE P.I. #0007561 OFFICE Environment/Location

DATE June 25, 2009

FROM Jeff Carr *JK*

TO Files

Georgia DNR Office of
Historic Preservation Division

JUN 26 2009

RECEIVED

SUBJECT GDOT Project CSHPP-0007-00(561), Clarke County;
P.I. #0007561 and HP-080728-001:
Survey Report

Attached is the Survey Report for the subject project prepared by New South Associates of Stone Mountain, Georgia. The Department has reviewed and concurs with this report. This document describes the Department's efforts to identify historic properties located within the proposed project's area of potential effects and the evaluation of all identified properties through the application of the Criteria of Eligibility to determine eligibility for inclusion in the National Register of Historic Places.

JTC/

cc: Rodney N. Barry, P.E., FHWA, w/attachment (Attn: Kelly Wade)
W. Ray Luce, Deputy SHPO, w/attachment
Northeast Georgia Regional Development Center, w/attachment
Willard Steele, Seminole Tribe of Florida, w/attachment

CONCUR: *W. Ray Luce* DATE: 7/9/09
W. Ray Luce, Deputy SHPO

cc: Kim Coley, GDOT District 1 Environmentalist
Julie Coco, New South Associates

COLEY
WILSON
RAMPSON
WILLIAMS
0007561

Department of Transportation
State of Georgia
One Georgia Center
600 West Peachtree Street, NW
Atlanta, GA 30308

New South Associates
6150 East Ponce de Leon Avenue
Stone Mountain, GA 30083



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APPENDIX B

PROPERTY INFORMATION FORMS

PROPERTY INFORMATION FORM

Property Identification: The proposed Inglewood Avenue Mill House District is also identified as Resource 1 on the project location map. The properties that make up this proposed district were not identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The proposed Inglewood Avenue Mill House District lies along Inglewood Avenue and a portion of the abandoned Georgia RR/CSX Transportation Rail Bed (this portion of the rail bed is included in the district), between South Peters Street and Old Winterville Road, on the east side of the City of Athens. The proposed district is just north of the Little Oak Street Neighborhood (see Figure 1).

Date(s) of Development: Based on tax record, Sanborn Fire Insurance map, Athens City Directory, historic aerial photography research, and field assessment, the proposed Inglewood Avenue Mill House District appears to have developed between 1914 and 1926. The Georgia RR/CSX Transportation rail spur first came through the area in 1874; however, Inglewood Avenue does not appear in Athens city directories until 1909, where it is originally listed as Elwood Avenue. The 1913 Sanborn Fire Insurance Map shows the White City Manufacturing Company cotton yarn mill located at the northwest corner of Elwood (Inglewood) Avenue and the Georgia Railroad (see attached graphic). At this time only nine dwellings were shown to exist. According to the 1914 Athens City Directory the name of the street had changed from Elwood to Inglewood Avenue and the White City Manufacturing Company and 11 residential dwellings were listed on the street. The 1918 Sanborn Map shows the White City Manufacturing Company and 13 dwellings along Inglewood Avenue (northeast of the railroad), and a fourteenth residence at 357 Inglewood Avenue (presently located at 357 Little Oak Street; see attached graphic). There is no mention of a mill located on Inglewood Avenue in the 1920 Athens City Directory, but there are 14 houses (including 357 Inglewood Avenue) listed. According to the 1923 Athens City Directory, the Bowen Crews Company yarn mill is listed at the location of the White City Manufacturing Company and 14 residential dwellings are located to the northeast of the railroad. The Bowen Crews Company is last identified in the 1926 Athens City Directory, after which no mill is listed at that location. In aerial photographs dating from 1938, the mill appears to be no longer extant, but the 14 residential dwellings located northeast of the railroad appear identical to the 1918 Sanborn Map (see attached graphic).

As hydroelectric power became viable during the first years of the twentieth century, small textile mills began springing up in and near the City of Athens. Around 1913, J.W. Ingle opened the White City Manufacturing Company, a small electrical powered yarn mill. 1904 and 1909 Athens city directories indicate that Mr. Ingle was employed with the Climax Hosiery Mill as a manufacturer. Prior to 1904, there is no record of Mr. Ingle in Athens, although the 1910 Federal Census shows that he was born in North Carolina where it is possible that he may have become conversant with the textile industry. The 1914 Athens City Directory lists five residents living on Inglewood Avenue as mill workers and one a superintendent (presumably of a local mill). There is no indication as

to which mill these six residents were employed at, but it is probable that they may have all worked for the White City Manufacturing Company. Four of the remaining residents in the directory were employed as a carpenter, a fireman, a plumber, and a guard. All of may have been positions at the White City Manufacturing Company or elsewhere. The final resident in the directory was employed as a hackman (a taxi driver). In the 1920 city directory, nine mill workers were listed as living on Inglewood Avenue, in addition to a plumber and a painter. Whether or not the White City Manufacturing Company employed these residents is unclear, but again, it is very likely. Also at this time, 357 Little Oak Street is first identified as mill worker housing. By 1923, the mill had become the Bowens Crews Company yarn mill, which employed eight of the residents living on Inglewood Avenue. The Climax Hosiery Mill employed five residents living on Inglewood Avenue, two worked at the Athens Manufacturing Company mill, and one was employed by the Athens Mattress Company. Another worker living on Inglewood Avenue was identified as a mill worker, but no association is given to any specific mill. Two painters, a plumber, and a watchman for the Athens Waterworks also lived on Inglewood Avenue in 1923. By this time, 357 Little Oak Street had become the residence of the Bowen and Crews Company superintendent. The 1926 Athens City Directory specifies that the majority of residents living along Inglewood Avenue worked at a local mill, although not necessarily at Bowen and Crews. All of the consulted Athens cities directories show that Inglewood Avenue remained exclusively inhabited by white working class individuals and families from 1913 through 1926.

Two circa 2002 properties have been constructed at 143 and 145 Inglewood Avenue, in the area where the mill once stood. Although these properties are new construction, their design is sympathetic to that of the surrounding district.

Description: The proposed Inglewood Avenue Mill House District encompasses 18 properties, 16 of which are contributing (see Table 1, below). Contributing properties within the district consist of single and double family dwellings, all of which are frame, Bungalow type mill housing, with the exception of 357 Little Oak Street, which is a Gable-Ell type. A portion of the Georgia Railroad Corridor is also included in the proposed district. Many of the contributing properties have had only minor alterations or repairs, including new roofs, siding, and windows. Some properties have small additions or enclosed or partially enclosed porches. In general, these alterations do not affect the overall scale and layout of the proposed district (Photos 1-16).

Setbacks along Inglewood Avenue are approximately ten feet deep, with the exception of 186 Inglewood Avenue, which is set diagonally on the lot with a 15 foot set back, and 185 and 187 Inglewood Avenue, which are located off the northern end of Inglewood Avenue along a small, private drive. The majority of the lots are long and narrow, with the exception of 120, 130, and 140 Inglewood Avenue, which have been subdivided to create the lot for 357 Little Oak Street. Also, 186 Inglewood Avenue is consists of a large lot, and 180 Inglewood Avenue is a very large, L-shaped lot. The northeast end of Inglewood Avenue dead ends in front of 186, and runs southwest where it intersects with

Little Oak Street, before ending at the intersection with Oconee Street. The street is somewhat narrow and does not appear to have changed in width or grade since it was originally laid out (Photos 17-18, 20).

The Georgia Railroad Corridor is a linear resource, although it is lacking in most general characteristics that would suggest the historic presence of a railroad. The section of the railroad bed within the proposed Inglewood Avenue Mill House District does not feature physical characteristics such as gravel, asphalt, concrete, rails, or ballast. This section of the rail bed has lost a majority of its definitive horizontal dimensions and vertical dimensions have been greatly diminished by the presence of widespread vegetative overgrowth (Photos 19-21). The portion of the rail bed that lies within the district is raised and remains at original grade.

The overall setting of the area has remained relatively unchanged since 1913. Although the rail bed has been abandoned and the road paved, the rail bed has not been encroached upon and the road has remained fairly narrow. Some commercial development has been introduced to the southwest, along Oconee and Oak streets; however, Inglewood Avenue has remained a quiet, residential neighborhood.

Table 1: Evaluated Properties Within the Proposed Inglewood Avenue Historic District (Resource 1)

Property	Date of Construction	Type	NRHP Recommendation
357 Little Oak Street	c. 1918	Gable-Ell	Contributing
120 Inglewood Avenue	c. 1914	Bungalow	Contributing
130 Inglewood Avenue	c. 1914	Bungalow	Contributing
140 Inglewood Avenue	c. 1914	Bungalow	Contributing
150 Inglewood Avenue	c. 1914	Bungalow	Contributing
154 Inglewood Avenue	c. 1914	Bungalow	Contributing
166 Inglewood Avenue	c. 1914	Bungalow	Contributing
170 Inglewood Avenue	1930	Bungalow	Contributing
180 Inglewood Avenue	c. 1918	Bungalow	Contributing
186 Inglewood Avenue	c. 1918	Bungalow	Contributing
185 Inglewood Avenue	c. 1918	Bungalow	Contributing
187 Inglewood Avenue	c. 1918	Bungalow	Contributing
175 Inglewood Avenue	c. 1914	Bungalow	Contributing
163 Inglewood Avenue	c. 1914	Bungalow	Contributing
153 Inglewood Avenue	c. 1914	Bungalow	Contributing
145 Inglewood Avenue	2002	Bungalow	Non-Contributing
143 Inglewood Avenue	2002	Bungalow	Non-Contributing
Georgia Railroad Corridor	1874	Railroad Corridor	Contributing

National Register Recommendation: The proposed Inglewood Avenue Mill House District is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The proposed Inglewood Avenue Mill House District was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or suggested as a result of background research on the project area, or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for

evaluating the property under Criteria B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The proposed Inglewood Avenue Mill House District was evaluated under Criteria A and C, and does appear to possess significance under Criterion A in the areas of community planning and development, and social history, and under Criterion C in the area of architecture.

The dominance of mill employees living in the neighborhood and the area's association with the White City Manufacturing Company and the Bowen and Crews Company between 1913 and 1926 makes the proposed district eligible under Criterion A in the areas of community planning and development and social history.

The proposed Inglewood Avenue Mill House District remains an unaltered and excellent example of early twentieth-century mill workers' housing in early twentieth-century Athens. The property also represents various house types identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings, and is representative of a significant trend in Georgia's architectural history making it eligible under Criterion C in the area of architecture.

Integrity: The proposed Inglewood Avenue Mill House District has been determined to possess integrity in the areas of location, setting, feeling, association, design, materials, and workmanship. The street, houses, lots and neighborhood have maintained their original locations and layout, while the setting of the area has changed very little since 1913. The proposed district still conveys the feeling and association of a historic, mixed working and middle class neighborhood. The houses associated with the mill village have undergone minimal alterations, maintaining a high degree of integrity in the areas of design, materials, and workmanship.

Proposed Boundary (Justification and Description): The proposed National Register boundary for the proposed Inglewood Avenue Mill House District corresponds to a visual boundary, incorporating all remaining buildings and structures historically associated with the mill worker housing along Inglewood Avenue and containing approximately 12.15 acres. The proposed district includes all legal parcels along Inglewood Avenue and 357 Little Oak Street (parcel number 172C1 J017). The boundary then follows Little Oak Street east down to the old Georgia Railroad road, south across Little Oak Street, and then west along the southern edge of the abandoned Georgia RR/CSX Transportation rail bed before crossing back over Little Oak Street at the southwest corner of parcel number 172C1 J002.

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens East Quadrangle Zone 17 S; Easting 227806; Northing 3681524.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project CSHPP-0007-00(561), Athens-Clarke County by:

Christina Olson
New South Associates

6150 E. Ponce de Leon Ave.
Stone Mountain, Georgia 30083
(770) 498-4155, ext. 126

References:

Athens City Directory

- 1889 M.C. Pope & Company, Athens, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1897/98 W.G. & M.E.R. Finney, Athens, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1904 Athens Banner Herald, Athens Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1909 Athens Directory Company, Athens, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1914/15 Hackney & Moale, Company Printers, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1916/17 Publishers Press, Atlanta, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1920/21 McGregor Company, Athens Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1923/24 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1926/27 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1928-29 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.

Georgia Historic Preservation Division

- 1991 *Georgia's Living Places: Historic Houses in Their Landscaped Settings and Commercial Types in Georgia.*

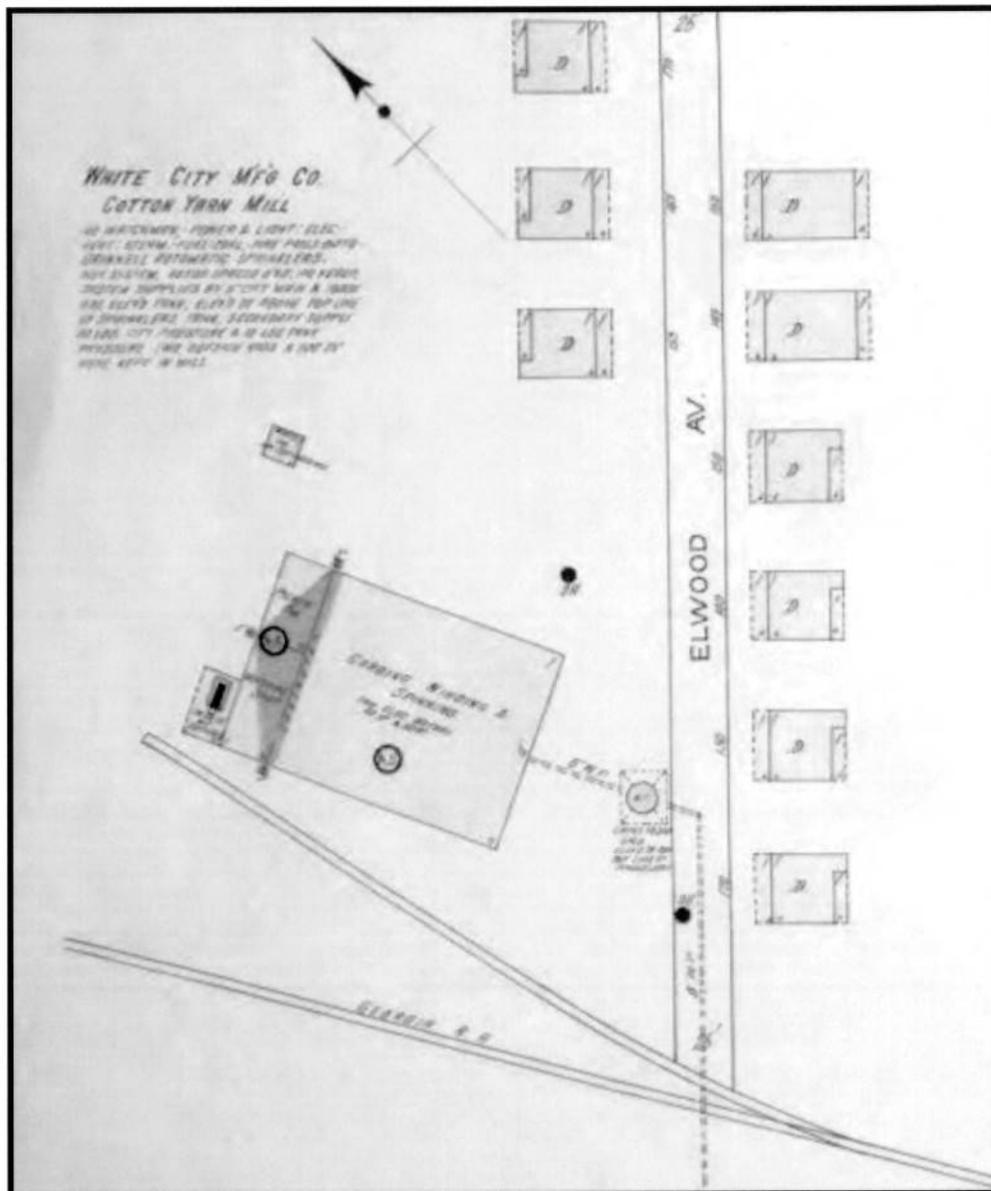
Sanborn Fire Insurance Maps

- 1913 Sheet 17. On file: University System of Georgia, Digital library of Georgia, GALILEO. <<http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1913/>>. Accessed: 05 Sep 2005.
- 1918 Sheet 24. On file: University System of Georgia, Digital library of Georgia, GALILEO. <<http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1918/>>. Accessed: 05 Sep 2005.

U.S. Federal Census Records

- 1910 On file: ancestry.com. <http://search.ancestry.com/cgi-bin/sse.dll?rank=1&gsfn=James&gsln=Ingle&=&_82004042__gpid=13&_82004043=Clarke&_82004044=&_8000C002=&_80008002=&_80018002=&ti=0&ti.si=0&gss=angs-c&pcat=USFEDCEN&h=3296067&recoff=1+3&db=1910USCenIndex&indiv=1>. Accessed: 05 Nov 2005.

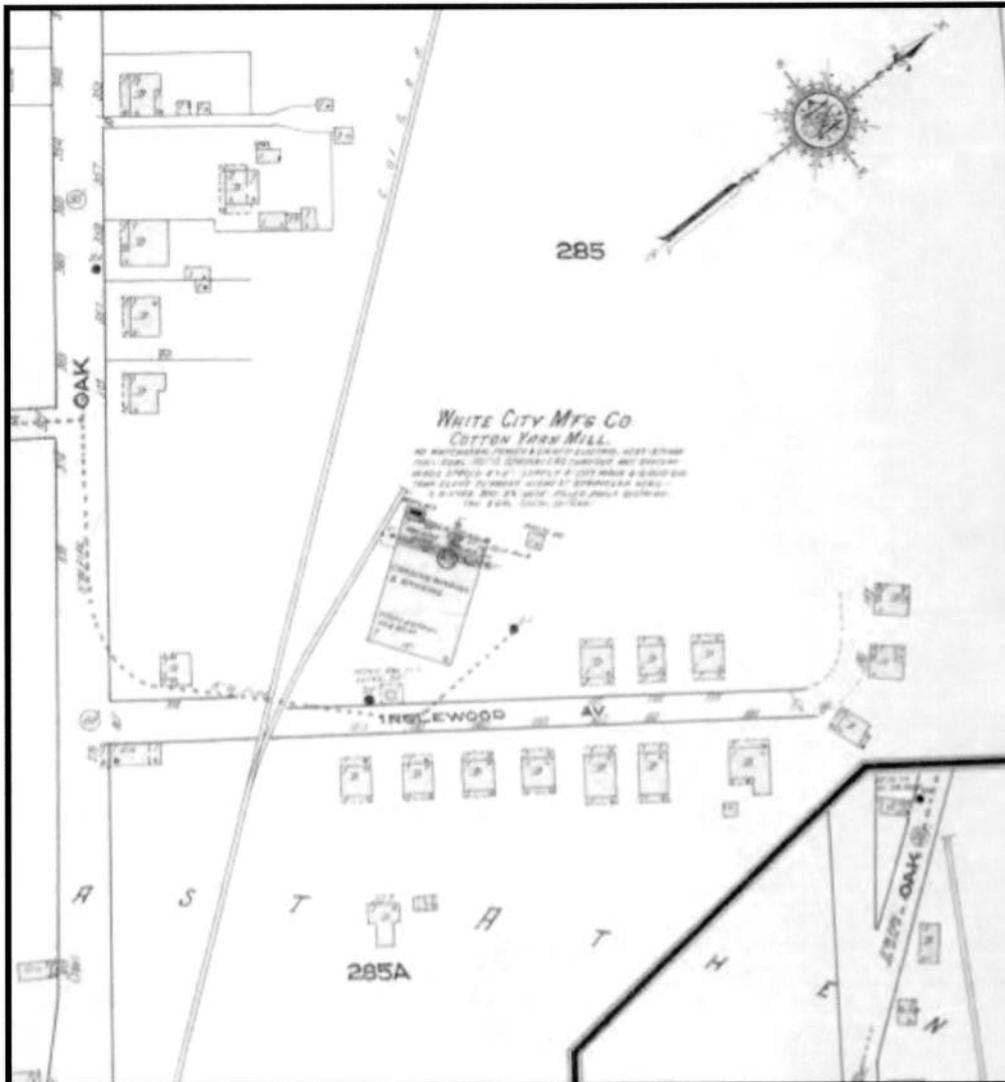
1913 Sanborn Fire Insurance Map for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: <http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1913/>

(Elwood Avenue Becomes Inglewood Avenue)

1918 Sanborn Fire Insurance Map for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



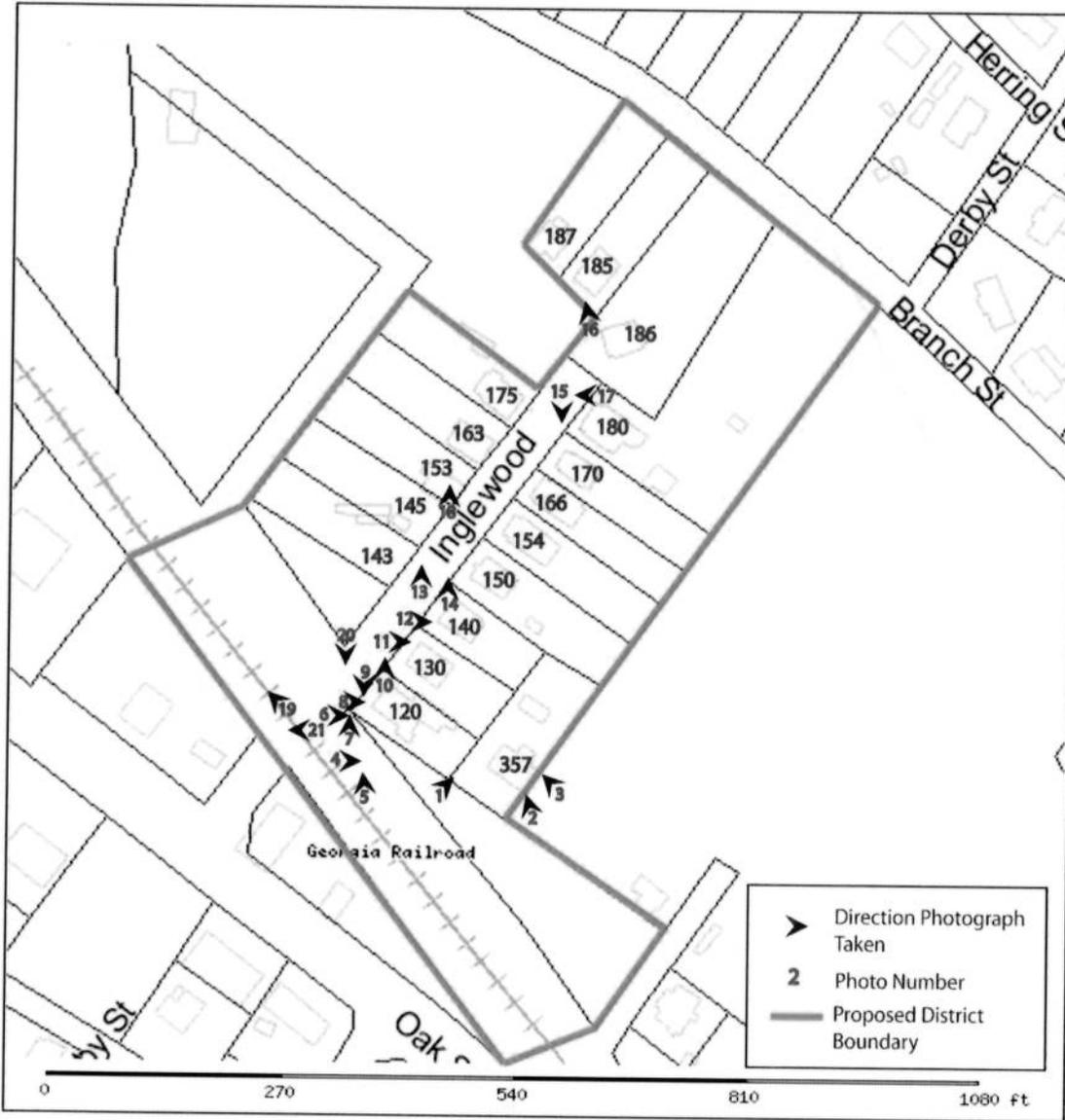
Source: <http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1918/>

1938 Aerial Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561



Source: Georgia State University Library

**Photo Key for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Source: 2006 Clarke County Parcel Map

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 1: 357 Little Oak Street, southwest (front) elevation



Photo 2: 357 Little Oak Street, south oblique

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 3: 357 Little Oak Street, southeast elevation



Photo 4: View looking east/southeast toward the drive for 357 Little Oak Street, from the intersection of Inglewood Avenue and Little Oak Street

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 5: View looking north from the intersection of Inglewood Avenue and Little Oak Street



Photo 6: View looking south/southeast from the intersection of Inglewood Avenue and Little Oak Street

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**

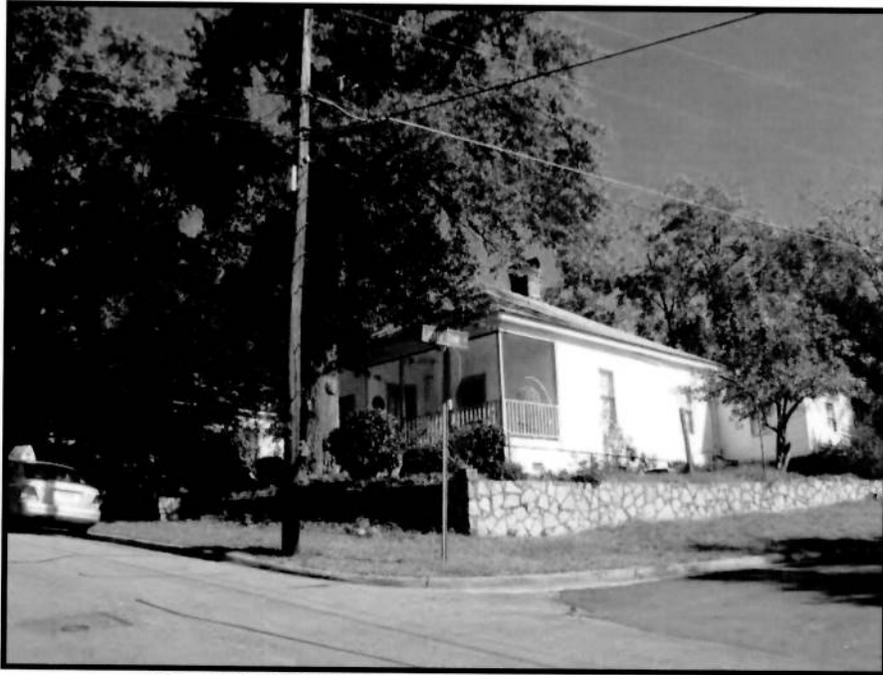


Photo 7: 120 Inglewood Avenue, west oblique

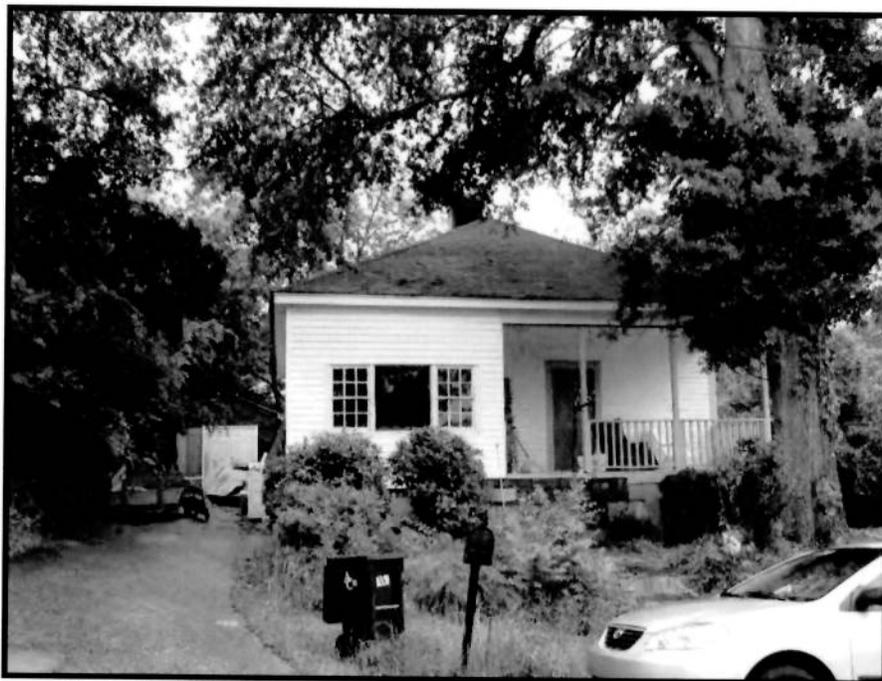


Photo 8: 120 Inglewood Avenue, northwest (front) elevation

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 9: 120 Inglewood Avenue, north oblique



Photo 10: 130 Inglewood Avenue, west oblique

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 11: 130 Inglewood Avenue, northwest (front) elevation



Photo 12: 130 Inglewood Avenue, north oblique

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 13: View looking northeast from in front of 130 Inglewood Avenue



Photo 14: View looking east/northeast from in front of 143 Inglewood Avenue

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 15: View looking south/southwest from in front of 166 Inglewood Avenue



Photo 16: View looking north/northwest from in front of 186 Inglewood Avenue

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 17: View looking west/southwest from in front of 186 Inglewood Avenue



Photo 18: View looking north/northeast from in front of 145 Inglewood Avenue

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 19: View of rail bed looking northwest from Inglewood Avenue



**Photo 20: View of rail bed looking south/southeast from in front of 120
Inglewood Avenue**

**Photography for the proposed Inglewood Avenue
Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 21: View looking west/northwest from intersection
of rail bed and Inglewood Avenue**

District Map and Proposed NRHP Boundary for the Inglewood Avenue Mill House District (Resource 1)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



PROPERTY INFORMATION FORM

Property Identification: The Tweedell House is also identified as Resource 2 on the project location map. This property was not identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The Tweedell House is located at 385 South Peter Street, south of the intersection of South Peter Street and the abandoned Georgia Railroad Corridor in Athens (see Figure 1).

Date(s) of Development: According to the tax assessor's record, the Tweedell House was constructed in 1950; the property is not listed in the Athens city directories until 1952. Although there is a house shown at this location on the 1918 Sanborn Fire Insurance Map, the footprint and location of the building on the lot do not match the current location of the Tweedell House. The central chimney and dual front entries make it likely that the residence was built as a late-period Saddlebag house type (whether or not the floor plan still functions as such is unknown). The rear porch appears to have been enclosed shortly after the house was constructed, circa 1960. The installation of aluminum siding appears to be a recent modification, circa 1995, as do the decorative metal columns on the front porch.

South Peter Street first appears in the 1897 Athens City Directory. Nearby surrounding properties have dates of development that range from 1874 to circa 2005. Adjacent properties ranging in date from circa 1914 to circa 2005.

Description: The Tweedell House appears to be a late Saddlebag house type with no identified academic style. The one-and-a-half-story frame building has a concrete slab foundation and aluminum siding (Photos 1-5, 7). An open front porch, constructed of concrete block with a poured concrete floor and a shed roof supported by decorative metal columns, almost spans the full width of the façade (Photos 1-2, 4, 7). There are dual front doors on the front elevation. The rear porch has been enclosed (Photos 3, 5). Windows are 3/1 sash on the visible elevations, with 6/6 sash windows in the roof gables (Photos 1-6). The lateral gable roof and the shed roofs of the front and rear porches are all clad in asphalt shingle and the main roof has a central, interior brick chimney (Photos 1-4). The rear porch of the house has been enclosed. The building is set back from the street approximately ten feet, atop a two to three foot embankment contained by a concrete retaining wall. A four-foot chain link fence encloses the property.

The setting of the Tweedell House is an urban residential neighborhood near an abandoned rail bed (Photo 9). There is a modern commercial area from Highway 78/Oconee Street to South Peter Street (approximately .12 of a mile) that ends in a commercial parking area across from the property on South Peter Street. The adjacent properties consist of an early twentieth-century residential building that has been converted into a public service facility, a late twentieth-century gas station, and an early twenty-first century parking and recreational vehicle facility.

National Register Recommendation: The Tweedell House is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The Tweedell House was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with events that have made a significant contribution to the broad patterns of our history, nor are there any known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or suggested as a result of background research on the project area or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for evaluating the property under Criteria A or B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The Tweedell House was evaluated under Criterion C and appears to possess significance in the area of architecture as a representative house type in Georgia's architectural history that is identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. Although the rear porch has been enclosed and aluminum siding has been installed, the original design of the house, the overall plan, and roof shape of the house have been retained. The installation of aluminum siding merely obscures the original siding materials and can be removed. Finally, the rear porch has also been enclosed, however this change does not overwhelm the building and is not readily visible from the public right-of-way.

Integrity: The Tweedell House has been determined to possess integrity in the areas of location, setting, feeling, association and design. The property is situated in its original location and has retained its traditional spatial and visual relationship to the streetscape and surrounding homes. Although the adjacent properties have undergone modern development and alterations these modifications have not directly impacted the house; thus, the overall setting and association of the property have been maintained. The building has also maintained the feeling of a late Saddlebag type house.

The Tweedell House has been determined not to possess integrity in the areas of materials and workmanship. The enclosure of the rear porch combined with the replacement of the original siding with a modern material and the replacement of the original front porch columns with decorative metal supports, has deteriorated the integrity of materials and workmanship.

Proposed Boundary (Justification and Description): The proposed National Register boundary for the Tweedell House corresponds to the legal property boundary (Parcel Number 172C1 A002), and contains .23 acres. The proposed boundary contains all National Register qualifying characteristics and features of the property and includes the house and the immediate surrounds. The proposed northwestern boundary corresponds to the retaining wall/fence fronting the property, which provides an intervening physical feature between the property's lawn and sidewalk (see attached boundary graphic).

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens East Quadrangle Zone 17 S; Easting 0281524; Northing 3759487.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project CSHP-0007-00(561), Athens-Clarke County by:

Christina Olson
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6150 E. Ponce de Leon Ave.
Stone Mountain, Georgia 30083
(770) 498-4155, ext. 126

References:

Athens City Directory

- 1889 M.C. Pope & Company, Athens, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1897/98 W.G. & M.E.R. Finney, Athens, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1904 Athens Banner Herald, Athens Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
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- 1914/15 Hackney & Moale, Company Printers, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1916/17 Publishers Press, Atlanta, Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1920/21 McGregor Company, Athens Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1923/24 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1926/27 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1928-29 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1942 Baldwin Directory Company, Inc., Charleston, South Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1947 Baldwin Directory Company, Inc., Charleston, South Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1949 Baldwin Directory Company, Inc., Charleston, South Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.

- 1952 Nelson's Baldwin Directory Company, Inc., Charleston, South Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.

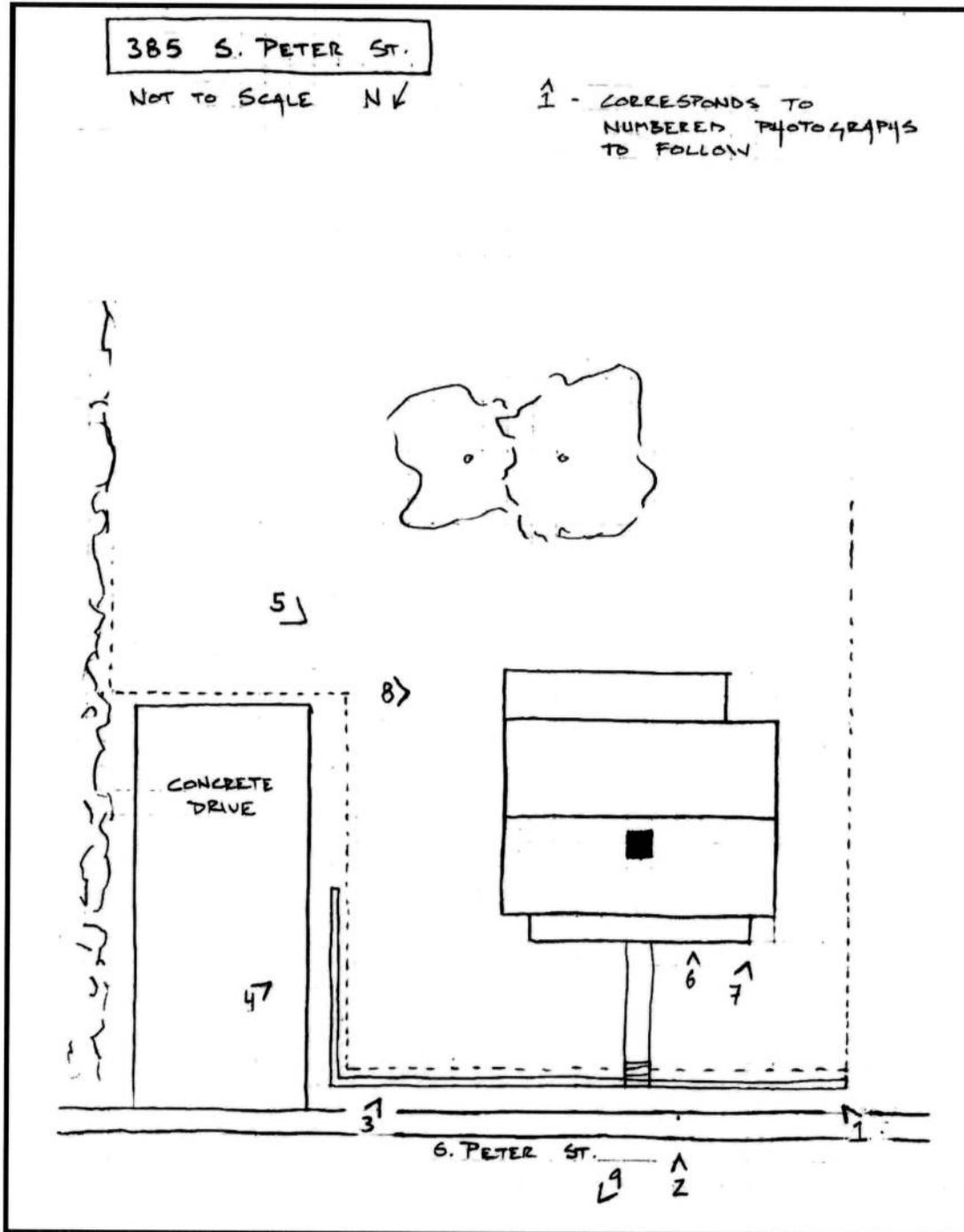
Georgia Historic Preservation Division

- 1991 *Georgia's Living Places: Historic Houses in Their Landscaped Settings and Commercial Types in Georgia.*

Sanborn Fire Insurance Maps

- 1918 Sheet 24. On file: University System of Georgia, Digital library of Georgia, GALILEO. <<http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1918/>>. Accessed: 05 Sep 2005.

Site Plan Sketch and Photo Key for the Tweedell House (Resource 2)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



**Photography for the Tweedell House (Reso
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0**



Photo 1: West elevation



Photo 2: Northwest elevation

**Photography for the Tweedell House (Resource 2)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 3: North oblique



Photo 4: Northeast elevation

**Photography for the Tweedell House (Resource 2)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 5: East oblique



Photo 6: Window, detail

**Photography for the Tweedell House (Re
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI#**



Photo 7: Front porch foundation

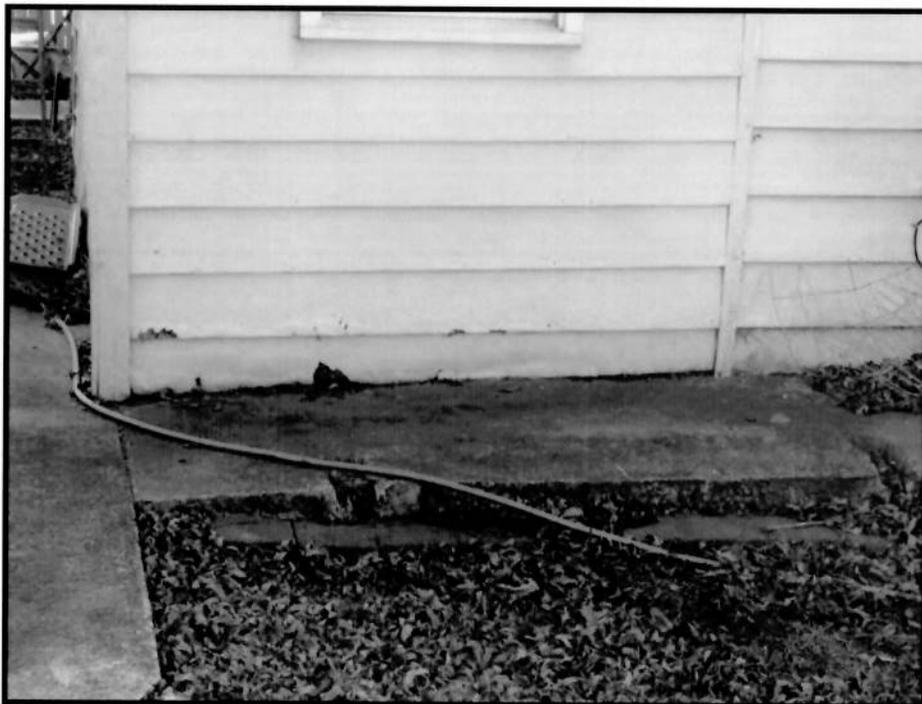


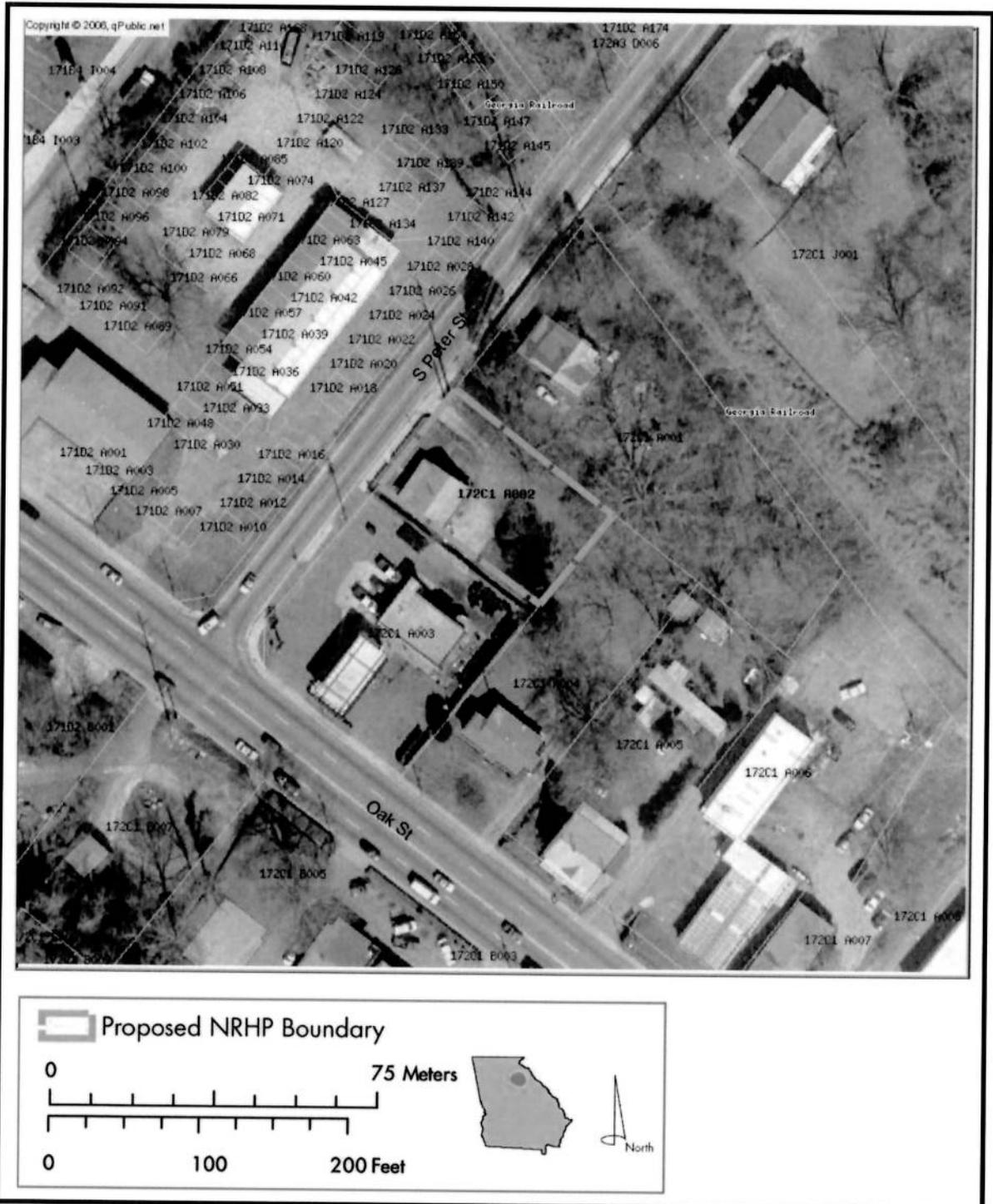
Photo 8: Foundation, detail

**Photography for the Tweedell House (Resource 2)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 9: View looking north/northeast along South Peter Street right-of-way,
from in front of the Tweedell House toward the rail bed**

**Proposed NRHP Boundary for the Tweedell House (Resource 2)
 GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561**



PROPERTY INFORMATION FORM

Property Identification: The Smith House is also identified as Resource 3 in the field notes and on the project location map. This property was not identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The Smith House is located at 375 South Peter Street, south of the intersection of South Peter Street and the abandoned Georgia Railroad Corridor in Athens (see Figure 1).

Date(s) of Development: According to the tax assessor's record, the Smith House was built in 1931; however, the property is identified in the 1914 Athens City Directory and on the 1918 Sanborn Fire Insurance Map at the same location and may precede the county tax assessor's build date by approximately 15 years. On the rear (southeast) elevation of the house there is what appears to be a circa 1930 addition, based on the 6/6 sash windows and hipped roof. There is also an L-shaped rear porch that was likely enclosed around 1995 - about the same time that the house was sided with vinyl. The property no longer serves as a residence and is currently occupied by the Advantage-Behavioral Health Systems Homeless Day Center.

South Peter Street first appears in the 1897 Athens City Directory. The nearby surrounding properties have dates of development that range from 1874 to circa 2005. Adjacent properties range in date from 1874 to circa 2005.

Description: The Smith House is a Folk Victorian style residence with a continuous foundation, which appears to be stucco over concrete block (Photo 6). The frame building has vinyl siding and an open front porch that almost extends the full width of the façade. The porch has a shed roof and turned wood posts (see Photos 1-3, 8-9). There are 1/1 wooden sash windows framing a central door on the symmetrical façade, and a 6/6 wooden sash window in the rear addition of the building (see Photo 4-5, 10). The two symmetrical front gables feature cornice returns, which continue into a cornice that is complete around the house eaves (including those on the rear addition). The front and central portion of the building is capped with a multi-gable roof, while the rear addition has a hipped roof and the enclosed rear porch is covered with an L-shaped shed roof (Photos 1-5, 7). The entire roof is clad in asphalt shingles, while a central brick interior chimney occupies the central ridgeline. There is a concrete retaining wall across the front of the property. The house is set back from the street approximately 30 feet. There is a large tree in the front yard and another in the back, both of which appear to be original plantings.

The setting is an urban residential neighborhood adjacent to an abandoned rail bed (Photo 11). There is a modern commercial area from Highway 78/Oconee St. to South Peter Street (approximately .12 of a mile) that ends in a commercial parking area across from the property on South Peter Street. The adjacent properties consist of a late nineteenth-century rail bed, a mid-twentieth century residential structure, and an early twenty-first century parking and recreational vehicle facility.

National Register Recommendation: The Smith House is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The Smith House was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with events that have made a significant contribution to the broad patterns of our history, nor are there any known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or suggested as a result of background research on the project area or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for evaluating the property under Criteria A or B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The Smith House was evaluated under Criterion C, and does appear to possess significance in the area of architecture. The property is representative of a house type in Georgia's architectural history as identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. The Smith House has maintained a fairly high degree of integrity in the area of design. Despite the rear addition, enclosed rear porch, and the installation of vinyl siding, the original design of the house is still clearly visible, as the rear alteration is not visible from the street. The house has retained significant features of the Folk Victorian style, such as the intersecting gable roof, porch configuration, and turned porch posts. Furthermore, the property has maintained its spatial relationship to the street, rail bed, and original lot layout of the neighborhood. Therefore, The Smith House is considered eligible for listing in the National Register under Criterion C for architecture.

Integrity: The Smith House has been determined to possess integrity in the areas of location, setting, feeling, association, and design. It is situated in its original location and has maintained its spatial relationship to the street, rail bed, and original lot layout; this combined with the overall retention of the original design have preserved the setting, feeling, and association of the property. Although some of the surrounding properties have undergone modern development and alterations and the house itself has been converted for use as a non-profit facility, these modifications have not directly impacted the physical characteristics of the building; thus, the overall setting and association of the property has been maintained. The property has also maintained the feeling of an early twentieth century Folk-Victorian style residence.

The Smith House has been determined not to possess integrity in the areas of materials and workmanship due to the installation of vinyl siding.

Proposed Boundary (Justification and Description): The proposed National Register boundary of the Smith House corresponds to the legal property boundary (Parcel Number 172C1 A001), and contains 0.59 of an acre. The proposed boundary contains all National Register qualifying characteristics and features of the property and includes the house and the immediate surrounds. The proposed northwestern border of the boundary corresponds to the retaining wall fronting the property, which provides an intervening physical feature between the property's lawn and sidewalk. (see attached boundary graphic)

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens East Quadrangle Zone 17 S; Easting 0281546; Northing 3759528.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project CSHP-0007-00(561), Athens-Clarke County by:

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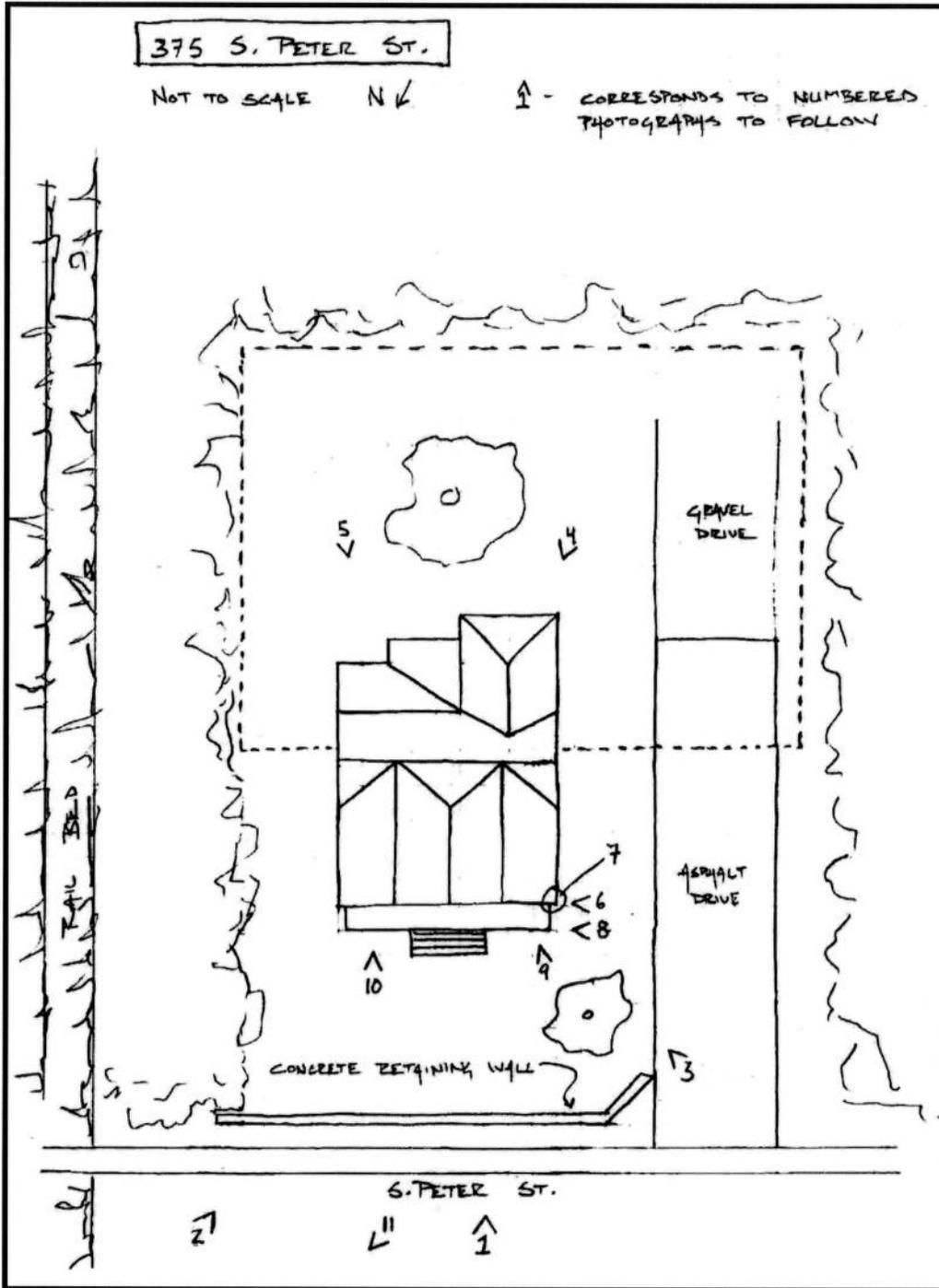
Georgia Historic Preservation Division

1991 *Georgia's Living Places: Historic Houses in Their Landscaped Settings and Commercial Types in Georgia.*

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Site Plan Sketch for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 1: Northwest (front) elevation



Photo 2: North oblique

**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 3: West oblique



Photo 4: South oblique

**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 5: Southeast (rear) elevation



Photo 6: Foundation, detail

**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 7: Front gable, detail



Photo 8: Front porch, detail

**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 9: Front porch, detail



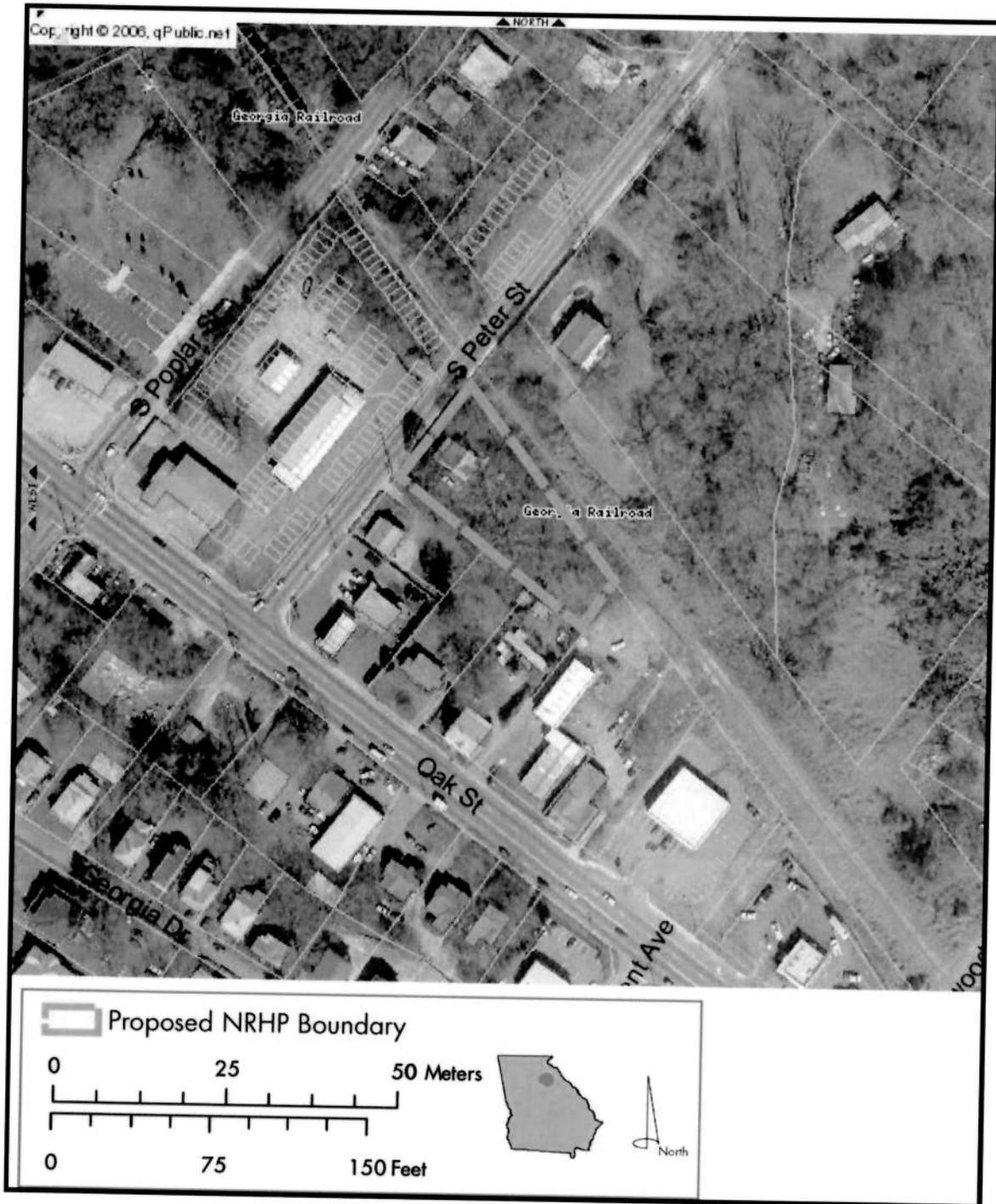
Photo 10: Window, detail

**Photography for the Smith House (Resource 3)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 11: View looking north/northeast along the South Peter Street right-of-way, from in front of the Smith House toward the rail bed

**Proposed NRHP Boundary for the Smith House (Resource 3)
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561**



PROPERTY INFORMATION FORM

Property Identification: The Fuller House is also identified as Resource 6 in the field notes and on the project location map. This property was not identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The Fuller House is located at 243 South Poplar Street, on the southeast side of the street just northwest of the abandoned Georgia Railroad Corridor in Athens (see Figure 1).

Date(s) of Development: According to the tax assessor's record, the Fuller House was constructed in 1930, but the residence first appears in the 1928 Athens City Directory. The aluminum siding appears to have been installed circa 1990.

South Poplar Street first appears in the 1909 Athens City Directory. The nearby surrounding properties have dates of development that range from 1874 to circa 2005. Adjacent properties date from approximately 1925 to 1930. Although Athens City Directory and Sanborn Fire Insurance Map research indicated that employees of the Climax Hosiery Mill occupied the Fuller House (Resource 6) shortly after its construction, there is no indication that the property was built in association with any specific mill.

Description: The Fuller House (Resource 6) is a Bungalow house type with Craftsman style influence. It is a frame building with aluminum siding and a brick pier foundation that has been in-filled with concrete block (Photos 1-3, 7). The windows are 6/6 wooden sash (Photo 5). It has a lateral gable roof with raised seam metal and exposed rafter tails in the eaves and knee braces in the gable ends (Photos 3-4). There is a, shed-roofed porch on the front (northeast) elevation that almost runs the width of the house (Photos 1-3, 6). The building's basement/crawlspace is partially exposed (Photos 1-2). The house is set back approximately 15 feet from the street and two large trees are located in the front yard. There was no access to the rear of the property due to fencing; therefore, assessment and photography of the property is based on the portion of the property visible from the right-of-way on Poplar Street.

The setting is an urban residential neighborhood. The property is approximately .12 of a mile northeast of a modern commercial strip located along Highway 78/Oconee Street. The Fuller House (Resource 6) is flanked on either side by early to mid-twentieth century residential structures. A late nineteenth-century rail bed and an early twenty-first century parking and recreational vehicle facility are located to the south (Photos 8-9).

National Register Recommendation: The Fuller House (Resource 6) is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The Fuller House (Resource 6) was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with events that have made a significant contribution to the broad patterns of our history, nor are there any known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or suggested as a result of

background research on the project area or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for evaluating the property under Criteria A or B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The Fuller House (Resource 6) was evaluated under Criterion C, and appears to possess significance in the area of architecture. The property is representative of the Bungalow house type and Craftsman style as identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings. The property has maintained a high degree of architectural integrity, retaining its original design and most of its original materials. Therefore, the Fuller House (Resource 6) is considered eligible for listing in the National Register under Criterion C for architecture.

Integrity: The Fuller House (Resource 6) has been determined to possess integrity in the areas of location, setting, feeling, association, and design. The property is situated in its original location and has retained its traditional spatial and visual relationship to the streetscape and surrounding homes. Although the adjacent properties have undergone some modern alterations, the overall setting, feeling, and association of the Fuller House (Resource 6) has been maintained. Despite a slight loss of integrity in the areas of materials and workmanship due to the installation of aluminum siding, the original design of the house is evident, as it has not been altered.

Proposed Boundary (Justification and Description): The proposed National Register boundary for the Fuller House (Resource 6) corresponds to the legal property boundary (Parcel Number 172A3 D004), and contains .17 acres. The proposed boundary contains all National Register qualifying characteristics and features of the property and includes the house, associated outbuildings and the immediate surrounds. The back edge of the sidewalk has been proposed as the northwestern border of the proposed boundary because the property's lawn extends uninterrupted to this point. The portion of the driveway within the right-of-way is not considered a contributing element of the property (see attached boundary graphic).

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens East Quadrangle Zone 17 S; Easting 281570; Northing 3759667.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project CSHPP-0007-00(561), Athens-Clarke County by:

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6150 E. Ponce de Leon Ave.
Stone Mountain, Georgia 30083
(770) 498-4155, ext. 126

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- 1920/21 McGregor Company, Athens Georgia, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1923/24 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
- 1926/27 Piedmont Directory Company, Miller Press, Ashville, North Carolina, publisher. On file: Hargrett Rare Book and Manuscript Library, University of Georgia, Athens, Georgia.
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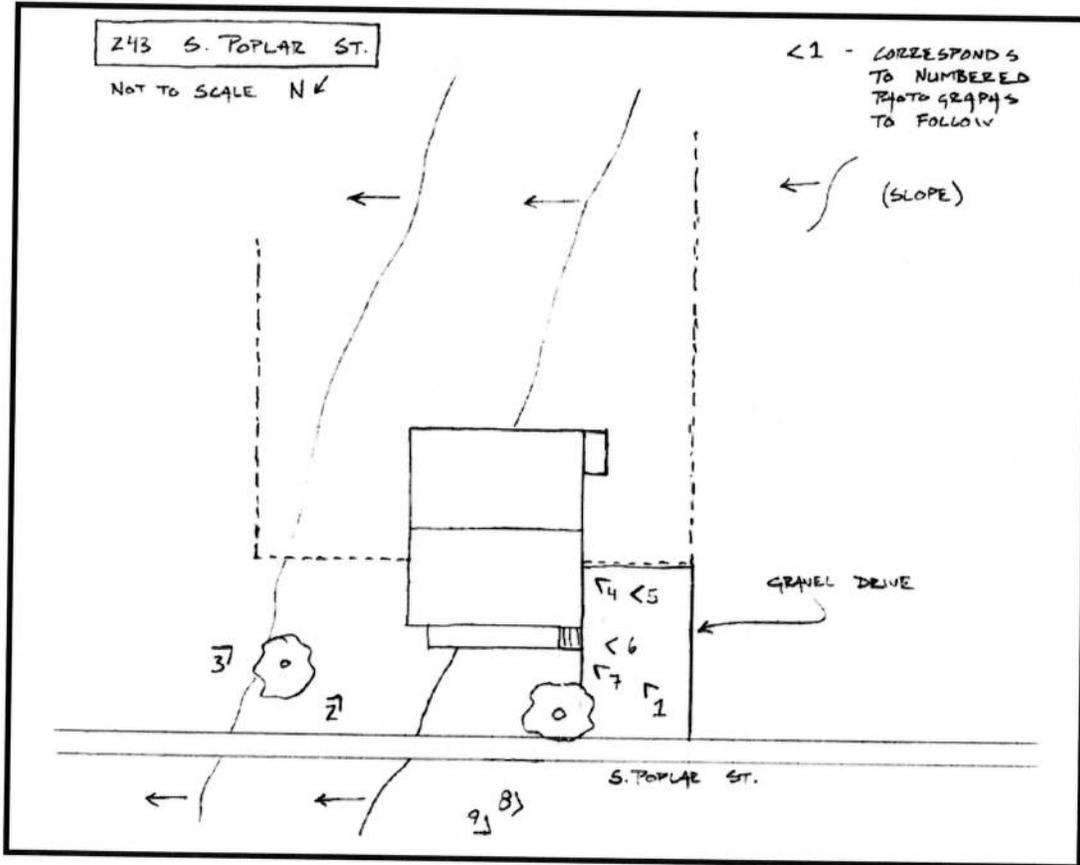
Georgia Historic Preservation Division

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Sanborn Fire Insurance Maps

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Site Plan Sketch and Photo Key for the Fuller House (Resource 6)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



**Photography for the Fuller House (I
GDOT Project CSHP-0007-00(561), Athens-Clarke County, P**



Photo 1: West oblique

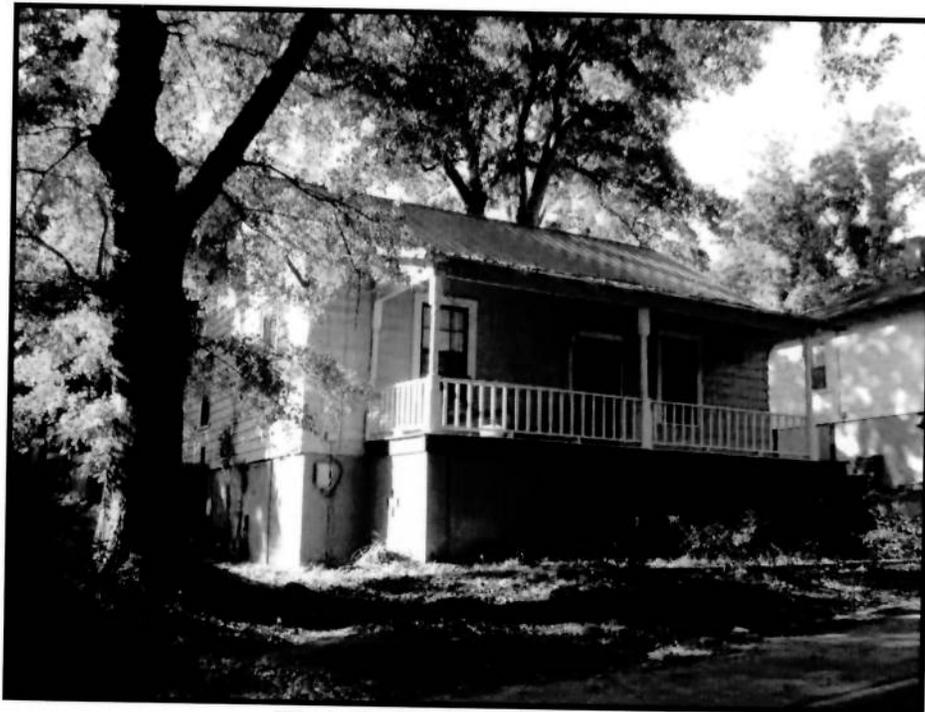


Photo 2: Northwest elevation

**Photography for the Fuller House (Resource 6)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**

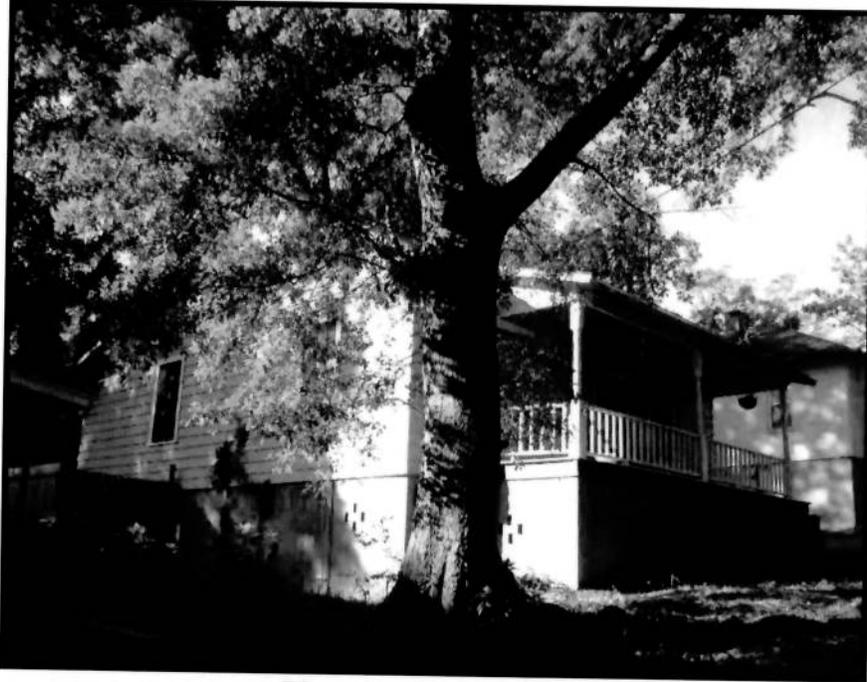


Photo 3: North oblique



Photo 4: Southeast gable, detail

**Photography for the Fuller House (Resource 6)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 5: Window, detail



Photo 6: Front porch, detail

**Photography for the Fuller House (Resource 6)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 7: Foundation, detail



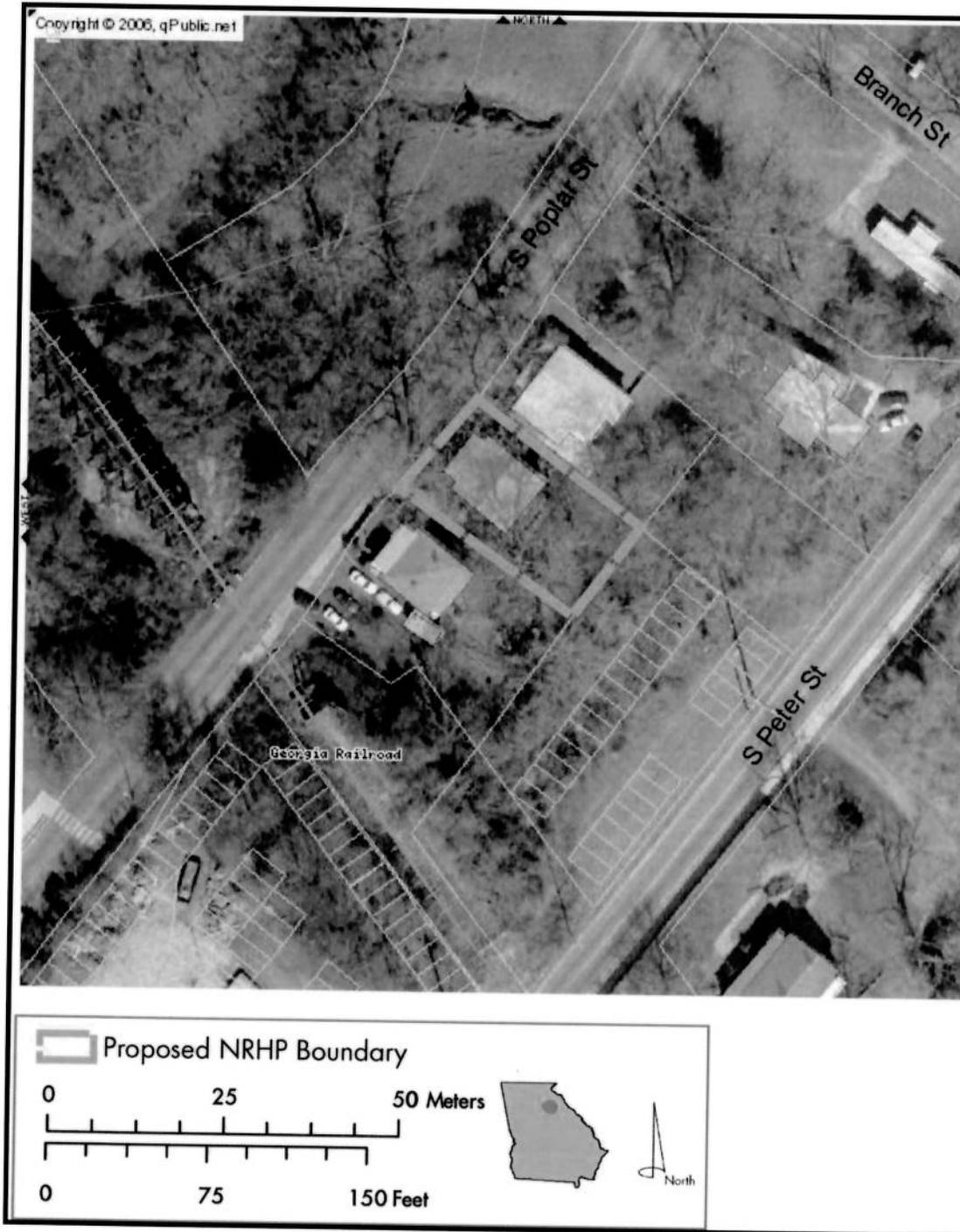
**Photo 8: View looking west on South Poplar Street right-of-way,
toward the rail bed and trestle**

**Photography for The Fuller House (Resource 6)
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 9: Detail of trestle, from in front of the Fuller House (Resource 6)

**Proposed NRHP Boundary for the Fuller House (Reso
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0**



PROPERTY INFORMATION FORM

Property Identification: The proposed Wilkerson Street Historic District is also identified as Resource 9 on the project location map. Neither this district, nor the properties that make up this district, were identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The Wilkerson Street Historic District (Resource 9) is located within the city limits of Athens, approximately .5 miles southeast of downtown. The proposed district boundaries roughly follow East Broad Street to the north, the North Oconee River on the east, and the Georgia Rail Road Corridor along the southeast (see Figure 1 and the attached boundary graphic).

Date(s) of Development: Based on Athens-Clarke county tax assessor records and Sanborn Fire Insurance maps, the residential buildings that make up the proposed Wilkerson Street Historic District (Resource 9) range in date from 1888 to 1950. The neighborhood likely developed as workers housing for those employed in the numerous industries located in the vicinity: the yarn manufacturing plant of the Athens Manufacturing Company (circa 1885) was located across the North Oconee River along East Broad Street, the Rucker Compress Company (circa 1888) was located at the intersection of East Broad Street and the Georgia Railroad, and the R.L. Bloomfield (also president of the Athens Manufacturing Company) Pottery was located at the intersection of Pottery and Wilkerson streets (circa 1888). Although the Rucker Compress Company and the Bloomfield Pottery appear to have been gone by the late 1920s, the Athens Manufacturing Company, along with several warehouses located in the area, remained in operation into the mid-twentieth century. The construction pattern and build dates of the houses located within the proposed Wilkerson Street Historic District (Resource 9) seem to mirror the dates of operations of industries located within the area.

Description: The proposed Wilkerson Street Historic District (Resource 9) encompasses 17 properties, 15 of which are contributing (see Table 1). Contributing property building types located within the district include two Gabled Ells, one Central Hall, one Saddlebag, one New South Cottage, one Pyramid Cottage, one Side-Gabled Cottage, five Bungalows, three American Small House types, and one commercial building (Photos 3-22, 25-28). In general, changes to houses in the proposed district have been limited to new roofs, siding, and windows; however, most of the contributing properties have had little or no alterations or repairs. Although the properties located at 181 and 173 Wilkerson Street have rear additions to facilitate multiple living quarters, the elevations visible from the public right-of-way have been maintained. There are four empty lots (two on the east side Pottery Street between East Broad Street and 175 Pottery Street, one at the northwest corner of Pottery and Wilkerson streets, and one at the southeast corner of the intersection of East Broad, Willow and Wilkerson streets). Concrete stairs or driveways indicate where the houses once stood.

Setbacks along Wilkerson, Pottery, and East Broad streets tend to be between five and ten feet, with some houses (123, 141 and 173 Wilkerson Street) having no setback. Lot sizes also vary, with about half of the contributing properties having square lots and the other half having double long lots. Wilkerson Street runs straight north and south through the middle of the district.

Pottery Street intersects Wilkerson Street at midpoint, running east and west for approximately 250 feet, before making a 90-degree turn north to East Broad Street. Although both streets are fairly narrow, Pottery Street is slightly narrower than Wilkerson Street.

The proposed Wilkerson Street Historic District (Resource 9) lies at the base of the raised Georgia Railroad Corridor, which effectively shields the neighborhood from modern commercial development to the southwest (Photos 29-30). Dudley Park lies to the southeast, across the North Oconee River (Photos 23-24). To the northwest of the district, along East Broad Street, the district lies adjacent to early and mid-twentieth century warehouses, as well as modern commercial and residential infill development (Photos 1-2, 26).

Table 1: Evaluated Properties Within the Proposed Wilkerson Street Historic District (Resource 9)

Address	Date of Construction	Type	NRHP Recommendation
820 E. Broad Street	circa 1888	Gabled Ell	Contributing
Eddie Craddock's Garage 890 E. Broad Street.	1975	Commercial (Lateral Gable)	Non-Contributing
920 E. Broad Street	circa 1903	Central Hall	Contributing
940 E. Broad Street	circa 1903	Gabled Ell	Contributing
122 Wilkerson Street	1946	American Small House	Contributing
175 Pottery Street	1946	American Small House	Contributing
170 Wilkerson Street	circa 1925	Bungalow	Contributing
180 Wilkerson Street	circa 1925	Bungalow	Contributing
184 Wilkerson Street	circa 1925	Bungalow	Contributing
186 Wilkerson Street	circa 1925	Bungalow	Contributing
Athens Economy Storage 187 Wilkerson Street	1973	Commercial (Butler Buildings)	Non-Contributing
181 Wilkerson Street	1940	American Small House	Contributing
173 Wilkerson Street	circa 1903	Saddlebag	Contributing
141 Wilkerson Street	circa 1913 circa 1918	New South Cottage Side-Gabled Cottage	Contributing Contributing
123 Wilkerson Street	1933	Bungalow	Contributing
1020 Wilkerson Street	circa 1918	Pyramid Cottage	Contributing
Weaver D's Restaurant 1016 East Broad Street	1950	Commercial (Front Gable)	Contributing

National Register Recommendation: The proposed Wilkerson Street Historic District (Resource 9) is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The proposed Wilkerson Street Historic District (Resource 9) was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or suggested as a result of background research on the project area, or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for evaluating the property under Criteria B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The proposed Wilkerson Street Historic District (Resource 9) was evaluated under Criteria A and C, and does appear to possess significance under Criterion A in the areas of community planning and development, and social history, and under Criterion C in the area of design.

The proposed Wilkerson Street Historic District (Resource 9) is considered eligible for listing in the National Register under Criterion A as a good example of a working class neighborhood that developed between the late eighteenth and mid-twentieth century. The various house types, although diverse within the proposed district, are typical of workers housing in Athens during this period of commercial and industrial development.

As a whole, the proposed district has maintained integrity under Criterion C in the area of architectural design. All of the commercial and residential building types found within the proposed Wilkerson Street Historic District (Resource 9) are identified in Georgia's Living Places: Historic Houses in Their Landscaped Settings and represent Georgia's architectural history during the late nineteenth and early half of the twentieth century. Although the original design of some individual properties have been diminished due to alterations, and despite the loss of some original buildings, the overall layout and character of the proposed district has maintained the integrity of a late nineteenth to mid-twentieth century neighborhood. The combination of house types and age physically conveys the heritage of Athens' working class through the evolution of the built environment. Therefore, the proposed Wilkerson Street Historic District (Resource 9) is considered eligible for listing in the National Register under Criterion C.

Integrity: The proposed Wilkerson Historic District has been determined to possess integrity in the areas of location, setting, feeling, association, design, materials and workmanship. The street, houses, lots and neighborhood have maintained their original locations and layout, while the setting of the area has changed very little since the mid-twentieth century. The proposed district still conveys the feeling and association of a historic, working class neighborhood. Although the design of the district has suffered some minor changes due to the loss of a few original buildings, the overall layout and plan of the neighborhood has been maintained. Furthermore, the overall integrity of materials and workmanship has been preserved as only some buildings have received new roofs, siding, windows, or rear additions.

Proposed Boundary (Justification and Description): The proposed National Register boundary for the proposed Wilkerson Street Historic District (Resource 9) corresponds to a visual boundary, incorporating all remaining buildings and structures historically associated with the neighborhood, and containing approximately 10 acres. The proposed district is roughly bounded on the north by East Broad Street, on the east by the North Oconee River, and along the southwest by the Georgia Railroad corridor. The district boundaries run approximately 512 feet along the northern edge, 743 feet along the eastern edge, and 859 feet along the southeast edge (see attached boundary graphic).

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens Quadrangle Zone 17 S; Easting 281230; Northing 3760007.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project MTA 00-T001-00(904), Clarke County by:

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(770) 498-4155, ext. 126

References:

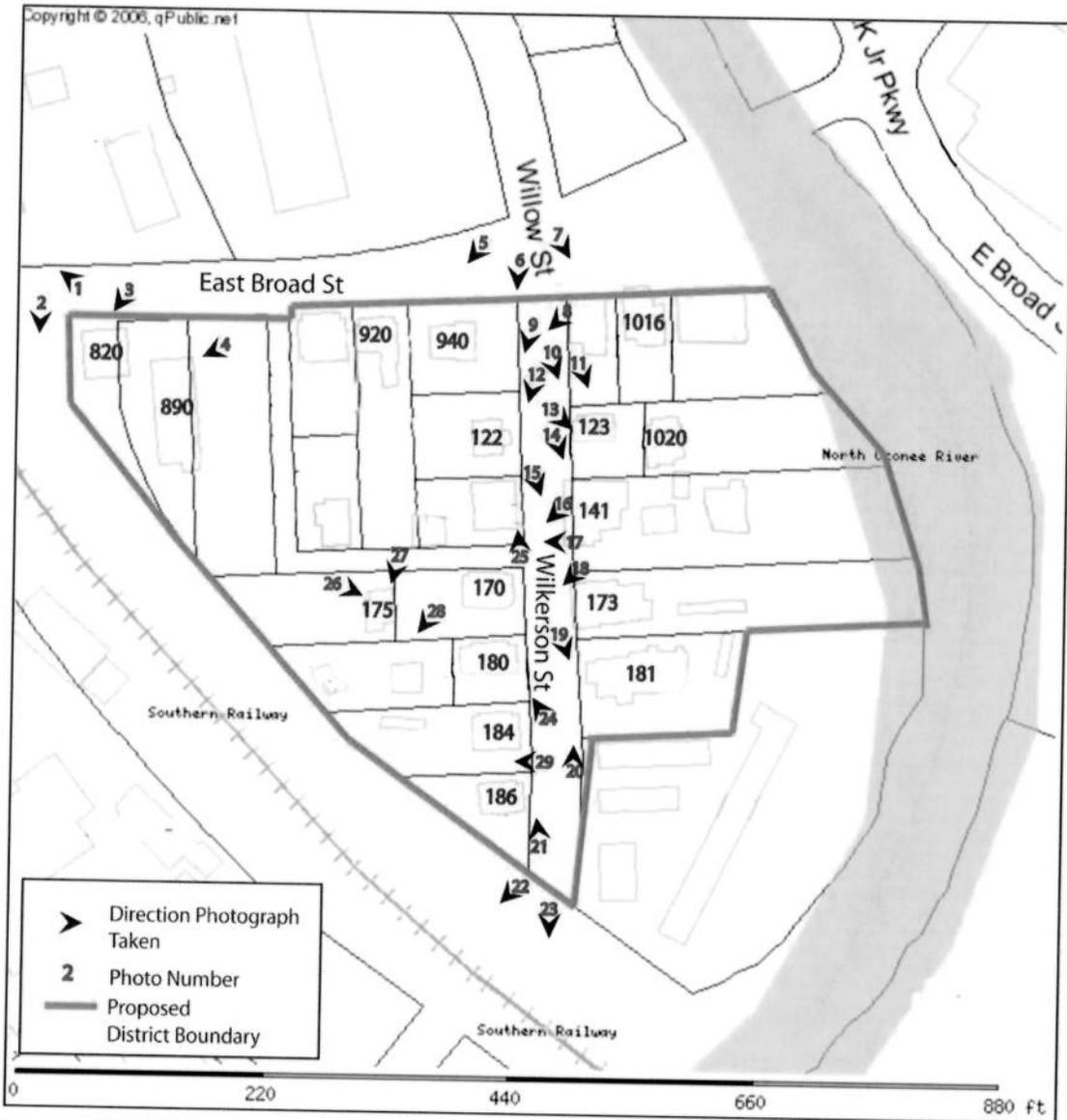
Georgia Historic Preservation Division

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- 1898 Sheet 3. On file: University System of Georgia, Digital library of Georgia, GALILEO. <<http://dlg.galileo.usg.edu/sanborn/CityCounty/Athens1898/>>. Accessed: 05 Sep 2005.
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**Site Map and Photo Key for the proposed Wilkerson Street
Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Source: 2006 Clarke County Parcel Map

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 1: View looking northwest toward Georgia Railroad corridor



Photo 2: View looking southwest along Georgia Railroad Corridor

Photography for the proposed Wilkerson Street Historic District (GDOT Project CSHPP-0007-00(561), Athens-Clarke County, E

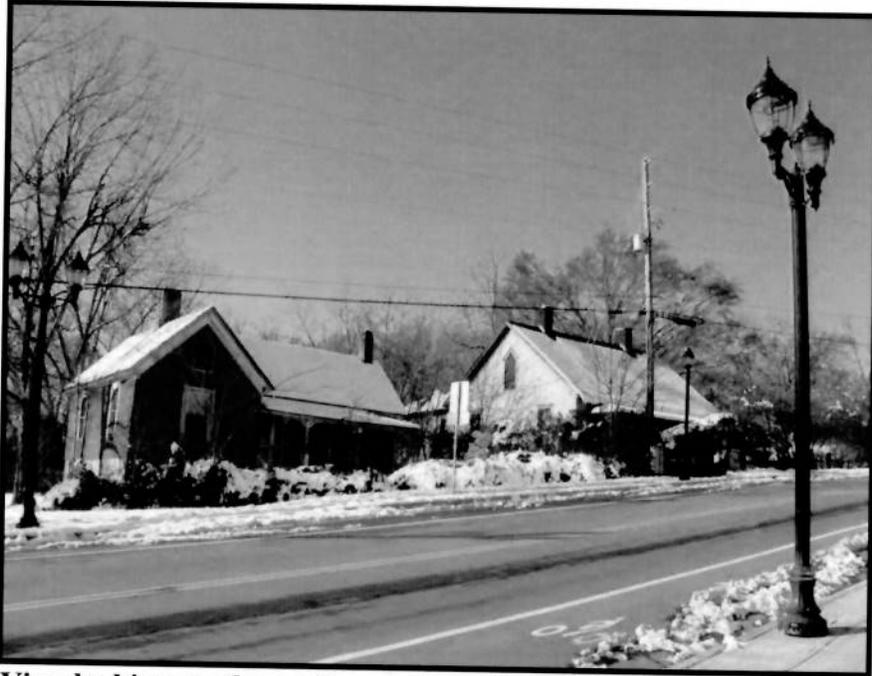


Photo 3: 820 East Broad Street, north oblique



Photo 4: Eddie Craddock's Garage (890 East Broad Street), northeast oblique

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 5: View looking southwest from intersection of East Broad and Willow streets,
northeast oblique of 940 and 920 East Broad Street**



**Photo 6: View looking south down Wilkerson Street
from the intersection of East Broad and Willow streets**

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 7: View looking southeast from corner of East Broad and Willow streets, northwest oblique of Weaver D's Restaurant, 1020 East Broad Street and 123 Wilkerson Street in background

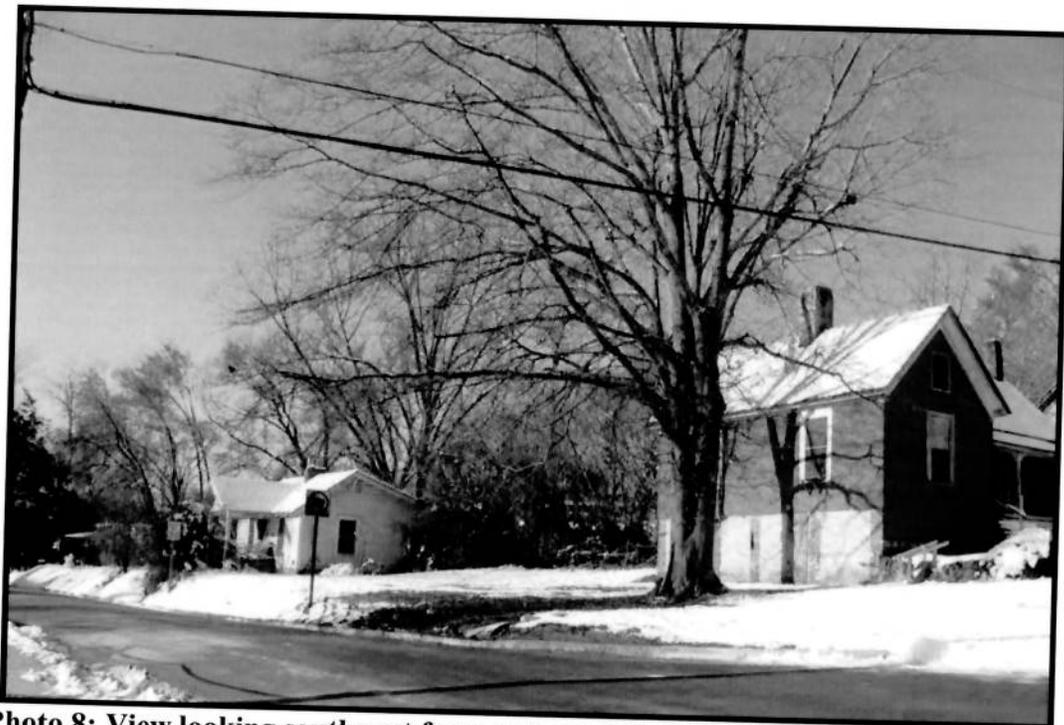
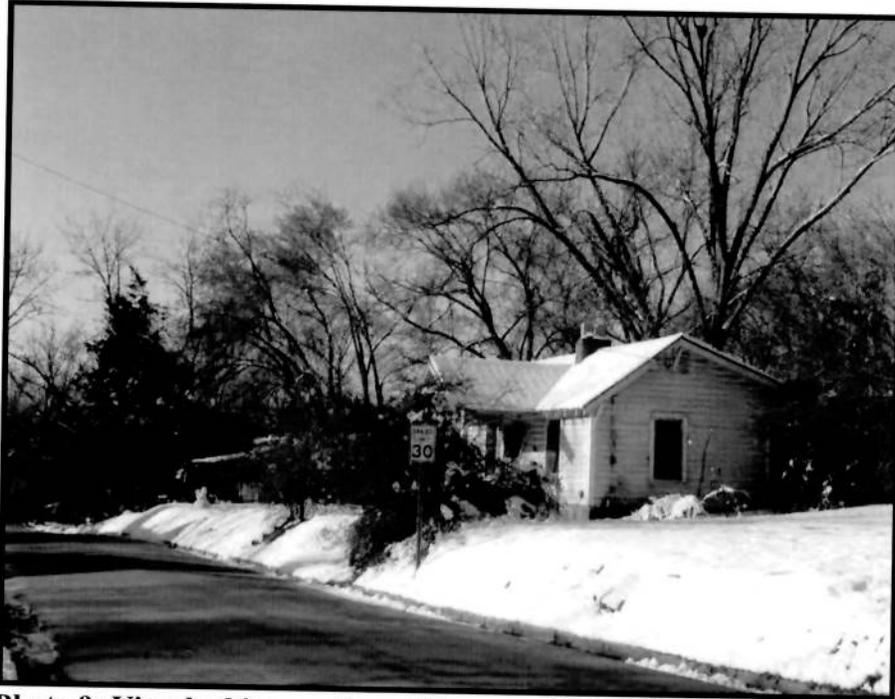


Photo 8: View looking southwest from corner of East Broad and Wilkerson streets, northeast oblique of 940 East Broad Street and 122 Wilkerson Street

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 9: View looking southwest from north end of Wilkerson Street,
northeast oblique of 122 Wilkerson Street**



**Photo 10: View looking south from north end of Wilkerson Street,
northwest oblique of 122 and 141 Wilkerson Street**

Photography for the proposed Wilkerson Street Historic District (GDOT Project CSHP-0007-00(561), Athens-Clarke County, I



Photo 11: View looking southeast from north end of Wilkerson Street



Photo 12: 122 Wilkerson Street, northeast oblique

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 13: 123 Wilkerson Street, northwest oblique



**Photo 14: View looking southeast from front of 122 Wilkerson Street,
Northwest oblique of 141 Wilkerson Street**

**Photography for the proposed Wilkerson Street Historic District (R
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI**



**Photo 15: View looking south from front of 141 Wilkerson Street,
Northwest oblique of 173 Wilkerson Street**



**Photo 16: View looking southwest from front of 141 Wilkerson Street,
northeast oblique of 170 Wilkerson Street**

**Photography for proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 17: View looking west toward project corridor from intersection of Wilkerson and Pottery streets

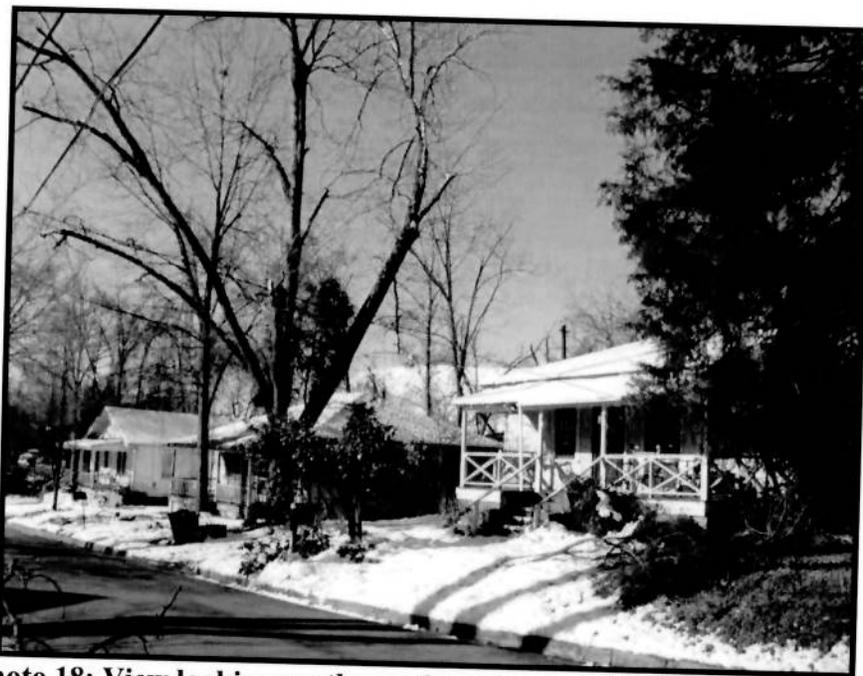


Photo 18: View looking southwest from front of 173 Wilkerson Street, northeast oblique of 180, 184, and 186 Wilkerson Street

Photography for the proposed Wilkerson Street Historic District (GDOT Project CSHPP-0007-00(561), Athens-Clarke County, GA)

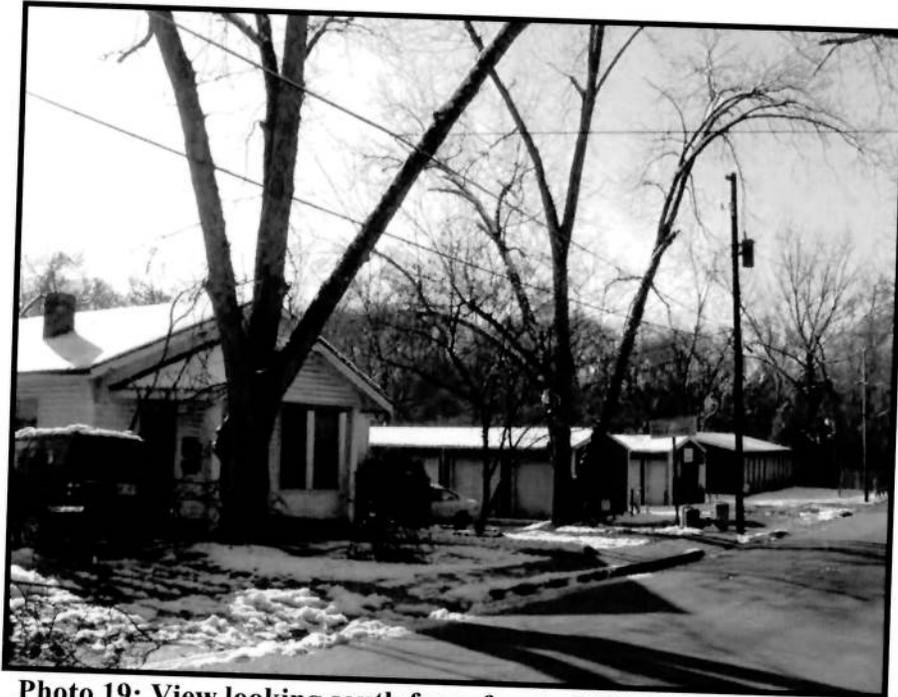


Photo 19: View looking south from front of 173 Wilkerson Street,

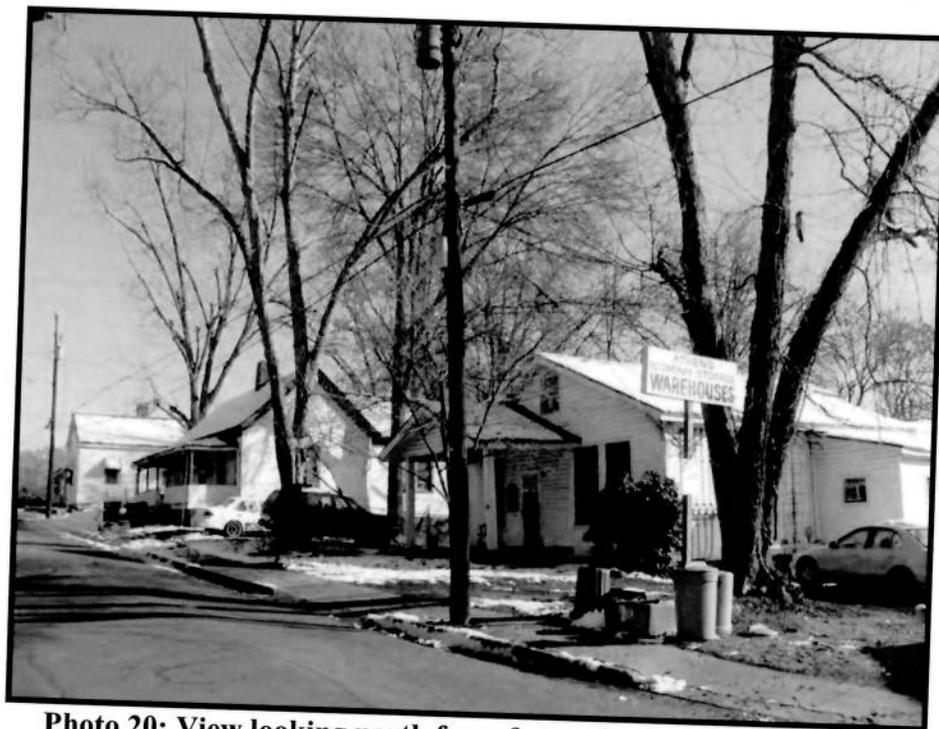
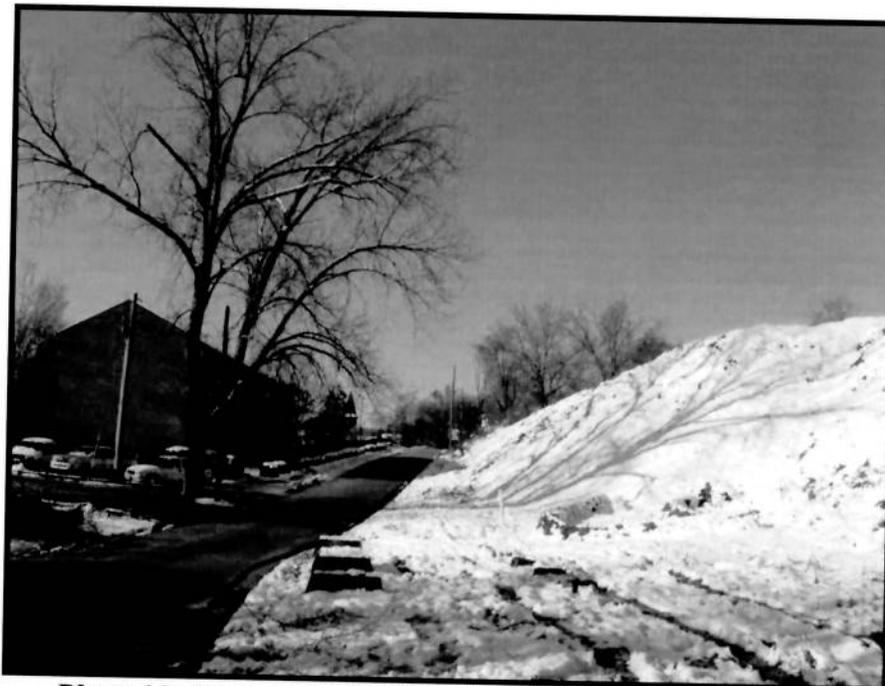


Photo 20: View looking north from front of 187 Wilkerson Street

**Photography for the proposed Wilkerson Street Historic District (Re
GDOT Project CSHP-0007-00(561), Athens-Clarke County, PI#**



**Photo 21: View looking north from south end of Wilkerson Street,
186 Wilkerson Street in foreground**

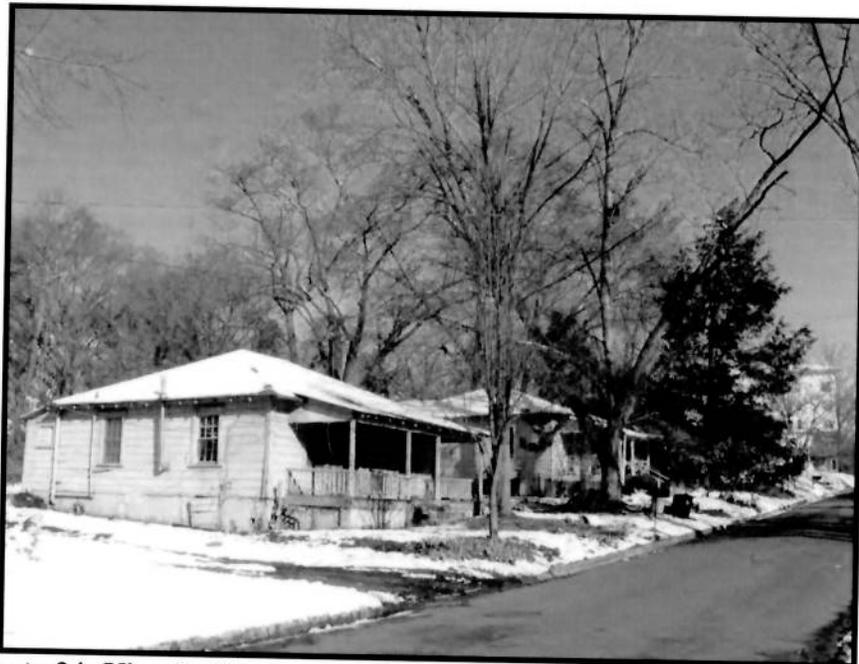


**Photo 22: View of project corridor, looking southwest from
south end of Wilkerson Street**

**Photography for the proposed Wilkerson Street Historic District
GDOT Project CSHP-0007-00(561), Athens-Clarke County**



Photo 23: View of project corridor, looking south from south end of Wilkerson Street



**Photo 24: View looking northwest from front of 184 Wilkerson Street
southeast oblique of 180 Wilkerson Street**

**Photography for the proposed Wilkerson Street Historic District (Res
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI#**



Photo 25: View looking north from intersection of Wilkerson and Pottery streets



Photo 26: 175 Pottery Street, northwest oblique

**Photography for the proposed Wilkerson Street Historic District (I
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, F**



Photo 27: 175 Pottery Street, front (north) elevation



**Photo 28: View of project corridor, looking southwest from rear of
170 Wilkerson Street**

**Photography for the proposed Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 29: View of project corridor, looking west between 184 and 186 Wilkerson Street

**Proposed NRHP boundary for the Wilkerson Street Historic District (Resource 9)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



PROPERTY INFORMATION FORM

Properties Identification: The Georgia Railroad Corridor and Contributing Properties are also identified as Resource 10 on the project location map. It is comprised of a rail corridor and its associated features, which include a partially demolished wooden trestle and masonry bridge bents, and a contiguous cluster of contributing warehouses. This resource was not identified in the 1974 DNR Clarke County Survey or in the 1988 DNR City of Athens survey.

Location: The Georgia Railroad Corridor and Contributing Properties (Resource 10) extend from the southwest side of Little Oak Street and Old Winterville Road, north to East Broad Street. It includes buildings bounded by East Broad Street, Oconee Street, Wilkerson Street and the Central of Georgia Railroad Corridor as well as the individual property located at 805 East Broad Street (see Figure 1).

Date(s) of Development: The abandoned Georgia Railroad spur line was constructed by the Central of Georgia Railroad & Banking Company in early 1874 as a means to connect downtown Athens to the Lula rail line. Ownership of the Central of Georgia Railroad line was transferred to the Louisville & Nashville in 1898. The Atlantic Coast Line acquired it in 1902 and remained the sole owner until 1967, when it merged with Seaboard Air Line Railroad to form the Seaboard Coast Line. In 1986, the railroad was reorganized as CSX Transportation. CSX abandoned the Athens spur in 1998.

The Georgia Railroad trestle remnant is the ruin of the 1883 railroad bridge constructed by Athens-based industrial investors William Dearing, Asbury Hull, William Williams, and James Camak. The trestle was constructed along with the North Oconee Bridge as part of an extension spur of the Georgia Railroad, bringing the terminus from Carr's Hill in East Athens into downtown, connecting with the Northeastern of Georgia line, and provided transport access to the cotton warehouses and mills that had developed in Athens during the mid-nineteenth century. The trestle first appears on the Sanborn Fire Insurance maps in 1915. It originally spanned approximately 725 feet from the southeast side of South Peter Street to the northwest side of Tail Creek. The portion of the trestle running between South Poplar Street and South Peters Street was demolished in 2000. Other portions of the current structure were also dismantled at that time.

The granite portions of the North Oconee Bridge masonry bents were constructed in 1883 as part of the railroad bridge. The concrete footings likely date from the 1940s. Much of the total structure was demolished in 2000 (ACC Online 2008; Jordan 2004).

The small number of the contributing warehouse buildings located along the Georgia Railroad corridor, just south of East Broad Street generally date from 1874 to 1950. According to Sanborn Fire Insurance maps, it appears that several cotton warehouses and related industries were in the area following the construction of the railroad spur. Modern commercial and industrial infill dating from the 1960s to the 1980s is located within the southeast corner of the group. The cluster is also abutted on the north and southeast by recent commercial and residential infill dating from 2000.

Description: The Georgia Railroad Corridor and Contributing Properties (Resource 10) is a linear resource that extends through the project area from the intersection with Old Winterville Road northwest to East Broad Street. The rail spur primarily consists of a raised bed and is lacking in most linear characteristics that would suggest the historic presence of a railroad, including such physical elements as gravel, asphalt, concrete, rails, or ballasts (Photos 1-4). Along with the loss of a significant amount of materials and general characteristics that contribute to the rail bed's horizontal dimensions, the vertical dimensions have also been greatly diminished by the presence of widespread vegetative overgrowth, which visually obscures much of the resource save for the associated trestle ruins and former North Oconee Bridge bents.

The trestle remnant on the Georgia Railroad corridor at South Poplar and South Peter streets consists of a timber structure, constructed of creosoted pilings resting on concrete footings (Photos 5-6). Three vertical piles flanked by diagonal piles create the pile bents, each supporting a pile cap. The structure is in a fairly advanced state of disrepair in part due to the 2000 demolition. Vestiges of the structure on the southeast side of South Poplar Street include two vertical timber piles and a timber retaining wall (Photo 7). A portion of a timber retaining wall and four concrete footings are also found along South Peter Street (Photos 8 and 9). The remnant of trestle bridge currently extends approximately 350 feet.

A total of six, tall granite bents and six concrete footings are all that remain of the former North Oconee Bridge on the Georgia Railroad corridor in the APE. Three bents each are located on both the north and south sides of the river and are partially covered in vegetation (Photos 10 and 11). The six concrete footings are adjacent to Wilkerson Road with three on each side (Photo 12).

Contributing property of the Georgia Railroad Corridor and Contributing Properties (Resource 10) includes a group of both masonry and frame warehouse buildings that line both sides of the Georgia Railroad corridor, East Broad Street, Oconee Street, and Wilkerson Street, and the railroad corridor roughly bound the cluster of contributing property. The commercial property located at 805 East Broad Street is also included (Photos 13-20).

The setting in the APE is that of an abandoned rail bed corridor and contributing warehouse property within an urban mixed-use area incorporating industrial, commercial, and residential development. The rail bed has been abandoned and the historic materials of the resource and its associated features have either been removed or partially demolished. Several of the early (circa 1900) masonry warehouses have been partially or completely razed, and modern infill has encroached on three sides (Photos 21 and 22).

National Register Recommendation: The property is considered **Eligible** for inclusion in the National Register of Historic Places.

National Register Criteria and Level of Significance: The proposed Georgia Railroad Corridor and Contributing Properties (Resource 10) was evaluated for eligibility for listing in the National Register using the National Register Criteria for Evaluation as outlined in 36 CFR Part 60.4. There are no known associations with individuals whose specific contributions to history can be identified and documented with this property. No associations were indicated or

suggested as a result of background research on the project area, or in any response to the Department's early consultation correspondence received from consulting parties. Therefore, there was no basis for evaluating the property under Criteria B. Also, there are no indications that the property is likely to yield information on important research questions in history or prehistory. This property does not appear to have the potential to be the principal source of important information. Therefore, there was no basis for evaluating the property under Criterion D.

The Georgia Railroad Corridor and Contributing Properties (Resource 10) was evaluated under Criteria A and appears to possess a state level of significance in the area of transportation, commerce, economics, industry, and community planning and development. The Georgia Railroad served as a major transportation route running from Augusta to Atlanta and historically has been an integral part of a larger rail network system that connected major shipping and transport hubs in the east to other major cities located in the interior southeastern United States. The contributing warehouse properties located at the north end of the corridor are associated with the industrial and commercial development of Athens and the city's dependence on cotton and textile manufacturing. The concentration of commercial and industrial development along the rail corridor underscores the importance of the railroad access for shipping and transport during the late nineteenth and early twentieth centuries. Therefore, the Georgia Railroad Corridor and Contributing Properties (Resource 10) are considered eligible under Criterion A.

The Georgia Railroad Corridor and Contributing Properties (Resource 10) was evaluated under Criterion C and appears to possess a state level of significance in the area of engineering and a local level of significance in the area of architecture. Despite the overgrowth of vegetation and loss of most character-defining features the raised railroad bed is still evident on the property. Remnants of the corridor's former bridge and trestle are also present. The contributing frame and brick commercial buildings are examples of warehouse design and construction dating from the late nineteenth and early twentieth centuries of Georgia's architectural history. Therefore, the Georgia Railroad Corridor and Contributing Properties (Resource 10) are eligible under Criterion C.

Integrity: The Georgia Railroad Corridor and Contributing Properties (Resource 10) has been determined to possess integrity in the areas of location, setting, design, materials, workmanship, feeling, and association. The location of the rail bed has not been significantly altered and the contributing warehouse properties have not been moved. Despite the presence of modern infill surrounding the rail corridor and contributing property, it remains an urban, mixed-use area and contains industrial, commercial, and residential development much as it did during the early twentieth century. Although the design and materials of the rail bed have been diminished, those of the trestle, masonry bridge bents and contributing buildings have been generally retained and the workmanship of these late nineteenth- and early twentieth-century properties and the feeling of a rail transport and shipping area is conveyed. The rail corridor and extant contributing properties also maintain their association with the history of transportation in the Georgia and the industry, economics, and commerce of the City of Athens.

Proposed Boundary (Justification and Description): The proposed National Register boundary for the Georgia Railroad Corridor and Contributing Properties (Resource 10)

corresponds to the existing railroad right-of-way and includes all character-defining features including the wooden rail trestle, masonry bridge bents, and the contiguous cluster of commercial warehouses bounded by East Broad Street, Oconee Street, Wilkerson Street and the Central of Georgia Railroad Corridor as well as the individual property located at 805 E. Street (Parcel Number 171B2 A006A). The proposed boundary contains approximately 1.13 acres and runs roughly 1.13 miles from the intersection of Little Oak Street and Wint Road, north to the northeast corner of Parcel Number 171B2 A006A, and roughly .15 miles to the west, from the eastern edge of Parcel Number 171B2 A006A west to Oconee Street (attached boundary graphic).

UTM Coordinates: 7.5 Minute Series Topographic Map. Athens Quadrangle Zone Easting 0281045; Northing 3760046.

Prepared: Completed pursuant to 36 CFR Part 800.4(c) in compliance with Section 106 of the National Historic Preservation Act for GDOT Project CSSHPP-0007-00(561) Clarke County.

Christina Olson
New South Associates
6150 E. Ponce de Leon Ave.
Stone Mountain, Georgia 30083
(770) 498-4155, ext. 126

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ACC Online

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Georgia Historic Preservation Division

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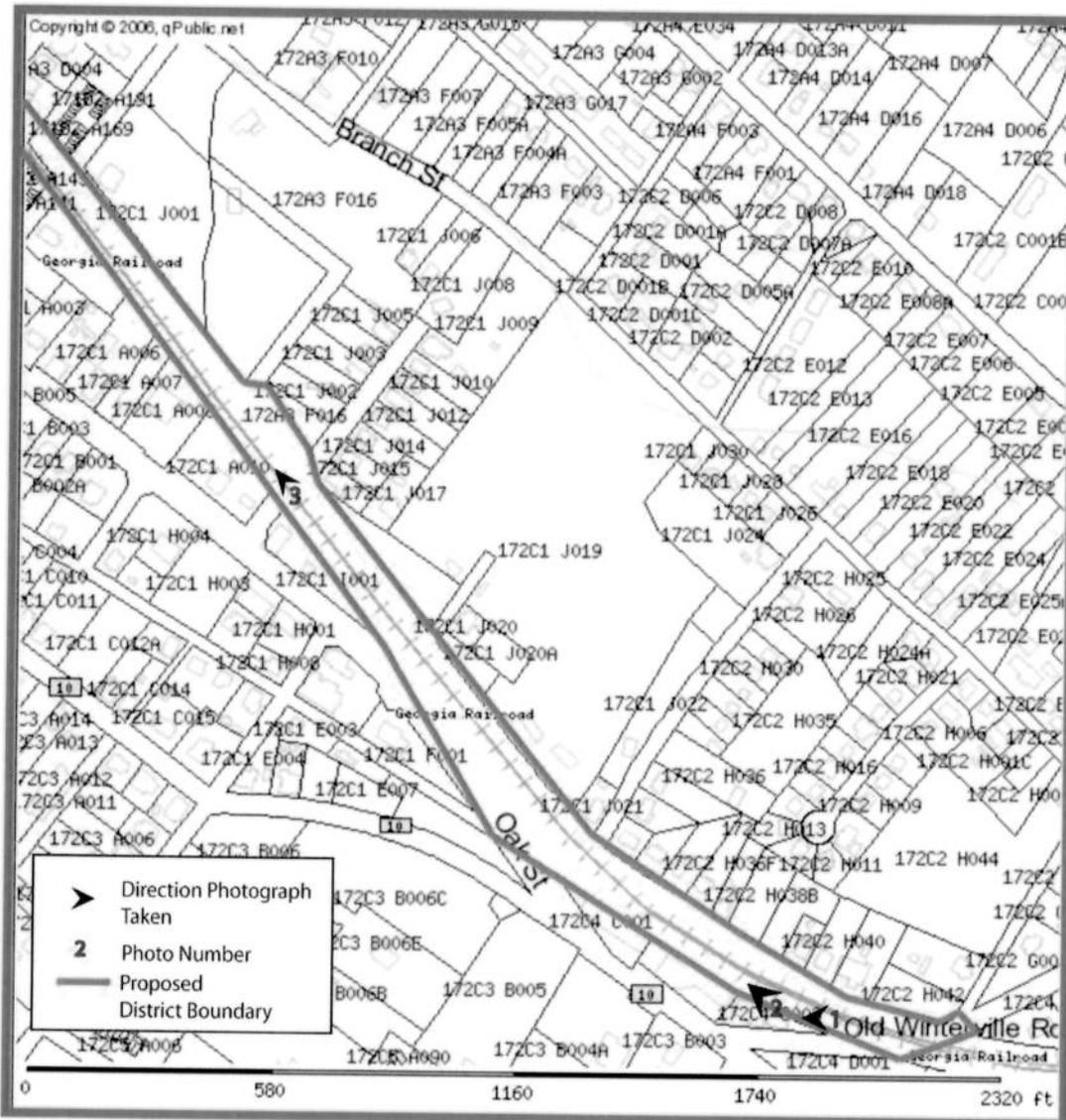
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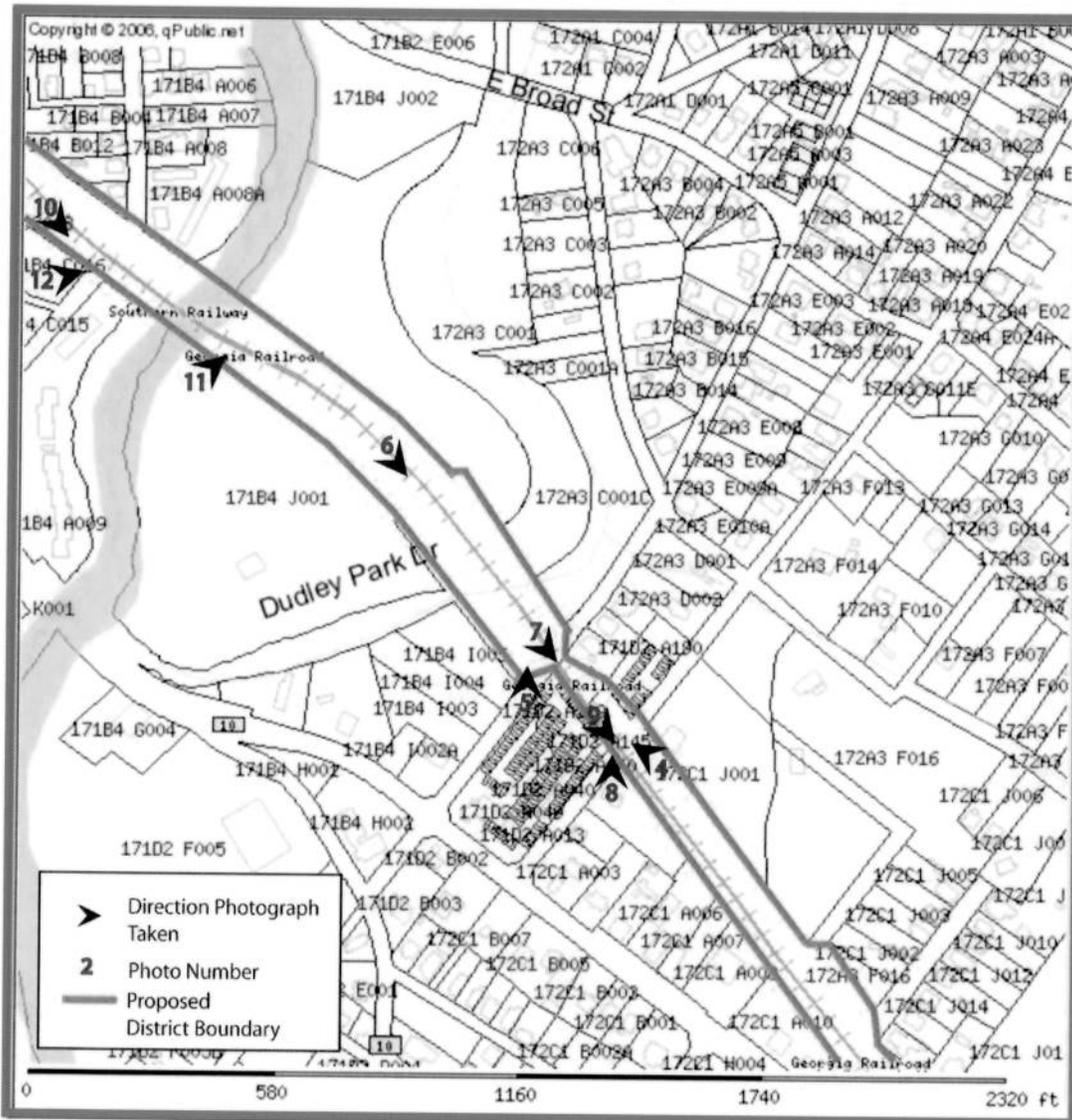
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Photo Key (1 of 3)
for the Georgia Railroad Corridor and Contributing Properties (Resource 10)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561



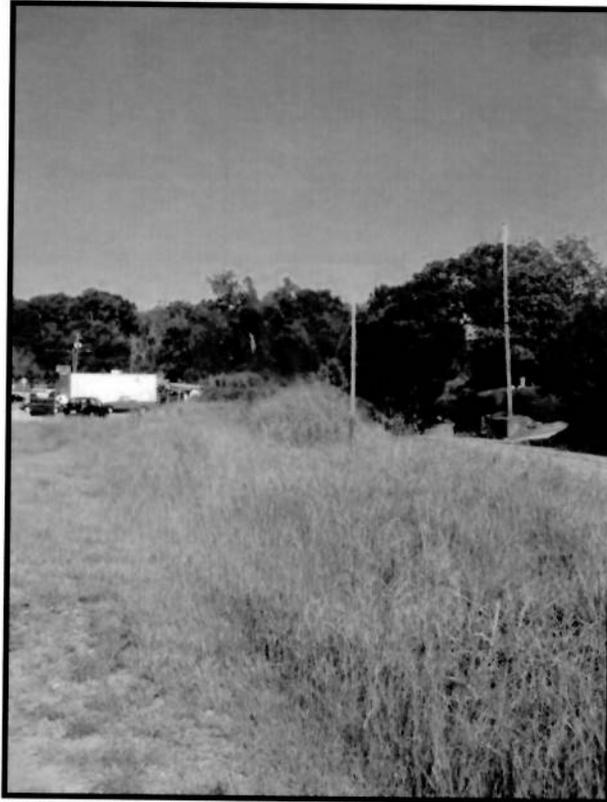
Source: 2006 Clarke County Parcel Map

Photo Key (2 of 2)
for the Georgia Railroad Corridor and Contributing Properties (Resource
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007)

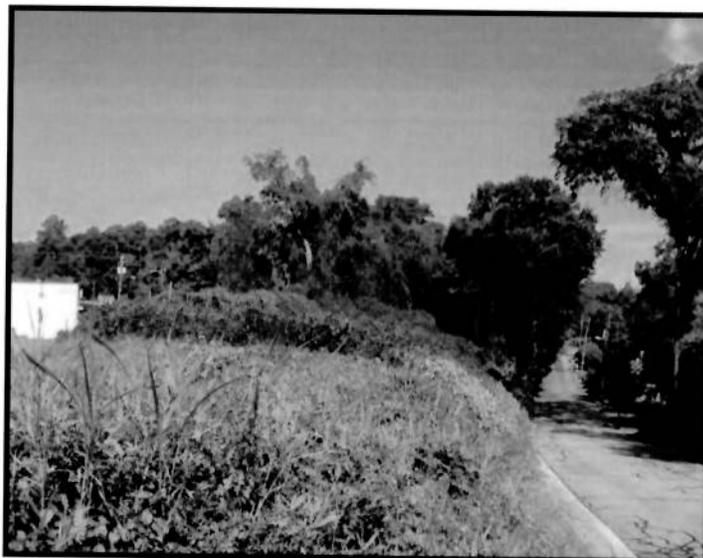


Source: 2006 Clarke County Parcel Map

**Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 1: View of rail bed facing northwest,
near intersection with Old Winterville Road**



**Photo 2: View of rail bed from Little Oak Street facing northwest,
near intersection with Old Winterville Road**

**Photography for Georgia Railroad Corridor and Contributing Properties (Re
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PE**



**Photo 3: Rail bed, view looking northwest from Inglewood Avenue,
near intersection with Little Oak Street**



Photo 4: Rail bed, view looking northwest from South Peter Street

**Photography for Georgia Railroad Corridor and Contributing Properties (Re:
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI**



Photo 5: Trestle, southwest oblique – view from South Poplar Street

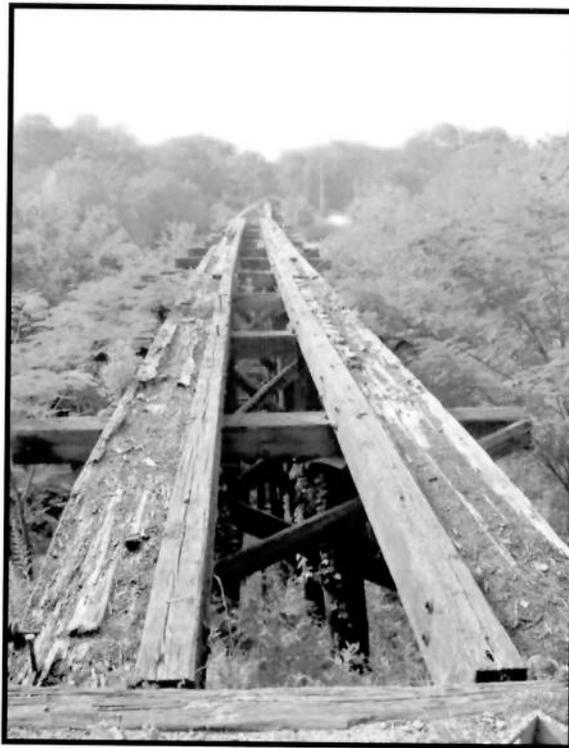


Photo 6: Trestle, detail – view from northern end of trestle to south

**Photography for Georgia Railroad Corridor and Contributing Properties (Re
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI**



**Photo 7: Trestle timber pilings and retaining wall, southeast side of South Poplar
view looking southwest**



Photo 8: Partial trestle timber retaining wall, northwest side of South Peter St

**Photography for Georgia Railroad Corridor and Contributing Properties (F
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, I**



Photo 9: Trestle concrete footings, northwest side of South Peter Street



Photo 10: View looking southeast from Wilkerson St.

**Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



Photo 11: Granite and concrete bent located in Dudley Park, view looking west

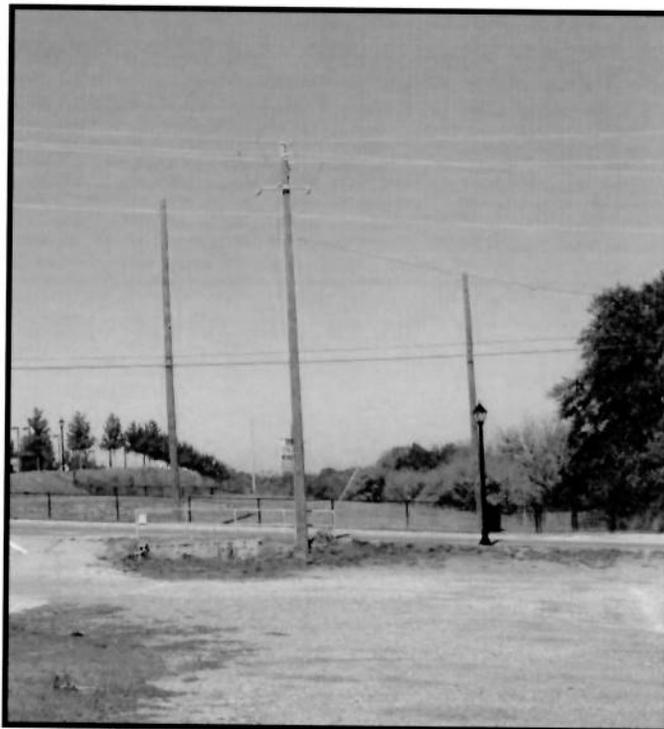


Photo 12: View looking southeast from Wilkerson St.

**Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 0007561**



**Photo 13: View of rail bed looking southeast from the east side of
Jittery Joe's Coffee Roaster**



**Photo 14: View of rail bed looking north from the east side of Jittery Joe's Coffee Roaster
Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)**



Photo 15: View looking east/southeast from the northwest edge of cluster, along East B Street



Photo 16: Jittery Joe's Coffee Roaster, northwest (front) elevation

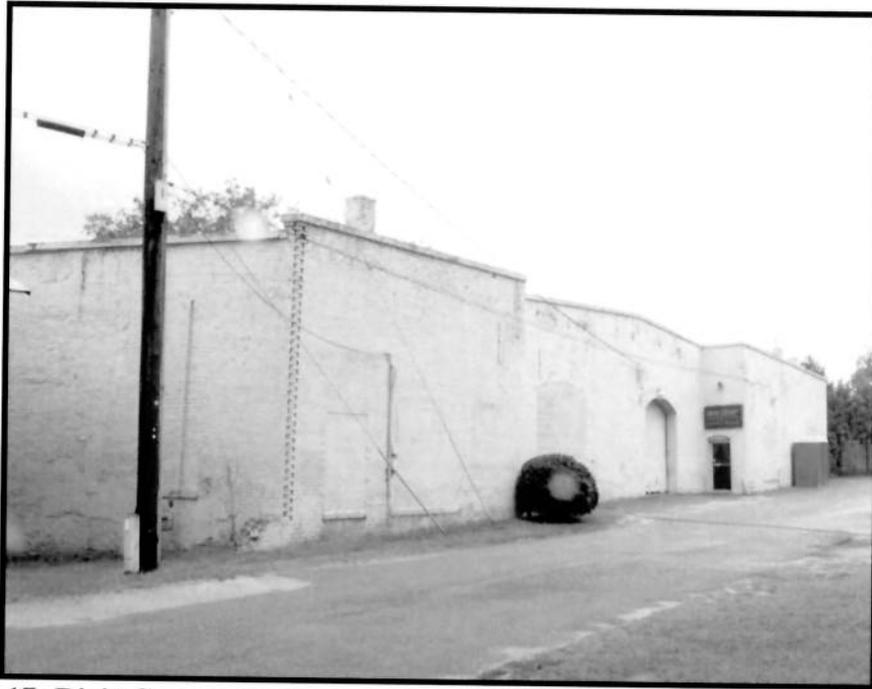
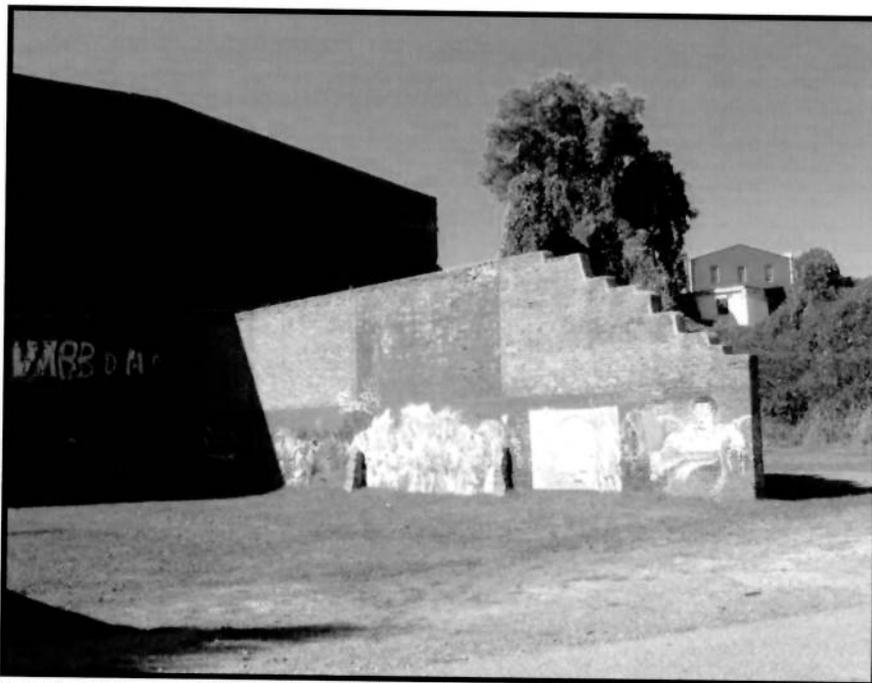


Photo 17: Dixie Canner Equipment Company, shipping and receiving building, northwest oblique



**Photo 18: View looking west/northwest from the west (front) elevation of the Dixie Canner Equipment Company, shipping and receiving building
Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)**



Photo 19: 805 East Broad Street, northwest oblique



Photo 20: View looking north from the rear (northeast elevation) Dixie Canner Equipment Company, shipping and receiving building
Photography for Georgia Railroad Corridor and Contributing Properties (Resource 10)

GDOT Project CSHPP-0007-00(561), Athens-Clarke County,



Photo 21: View looking east along East Broad Street from in front of Jittery Jo Roaster



Photo 22: View looking west along East Broad Street from in front of Jittery Jo Roaster

Proposed NRHP

for the Georgia Railroad Corridor and Contributing Properties (Resource
GDOT Project CSHPP-0007-00(561), Athens-Clarke County, PI# 000'



APPENDIX C
SUMMARY TABLE

Table 2: Resource Summary Table

Resource	Physical Destruction/Damage/Alteration	Isolation/Alteration of Setting	Visual/Audible/Atmospheric	Indirect
Inglewood Avenue Millhouse District	No Adverse Effect	No Effect	No Effect	No Effect
Tweedell House	No Effect	No Effect	No Effect	No Effect
Smith House	No Effect	No Effect	No Effect	No Effect
Fuller House	No Effect	No Effect	No Effect	No Effect
Wilkerson Street Historic District	No Effect	No Effect	No Effect	No Effect
Georgia Railroad Corridor and Contributing Properties	No Adverse Effect	No Effect	No Effect	Adverse Effect

APPENDIX D
PUBLIC COMMENTS

Rail to Trail – The Georgia Railroad Project
Responses to Citizen Input Questionnaire
November 2008



O'BRIEN & GERE

- 1) Project Goal – What is your vision for the proposed trail?
 - a. Add to the existing N Oconee River Greenway Network.
 - b. To provide a level, readily accessible corridor linking East Athens with downtown, multi-modal center, and ACC parks.
 - c. Provide visually exciting trail that becomes the hub of the N Oconee Greenway Network.
 - d. Restoration of this historic corridor including bridges into functional, attractive, safe bicycle, walking facility.

- 2) What are your top 3 positive impressions of the site?
 - a. Historic GA Railroad wooden trestle crossing
 - b. N Oconee River Crossing
 - c. Views of Athens from Dudley Park
 - d. Steady grade connection from downtown to Eastside

- 3) Negative impressions?
 - a. Devastation of berm and area between Peter & Poplar Streets
 - b. Feeling of diminishing safety as one approaches eastern terminus of trail

- 4) What are your concerns regarding the three bridge crossing locations? Are your concerns largely functional or is it your desire that any design emphasize historical accuracy?
 - a. No particular concerns for the N Oconee and Peter Street crossings, Historical accuracy is a good idea but neither of these two crossings need to reflect their former selves. Prefabs could look "great", but, the historic wooden trestle should be kept intact and preserved whether it is structurally sound or not. Wooden trestles are in-use all over the US and Canada, maintenance is doable and we need to find a way to save the structure.
 - b. Utmost importance should be given to the "reservation" of both historic structures and their "exact" topography/grades.

- 5) Do you have any specific environmental or preservation concerns about any portion of the proposed site? Historical views or sights?
 - a. Preservation of historical wooden trestle. It (the wooden trestle) will be the "focus" and "jewel" of the project, and a "symbol for the City of Athens".
 - b. With current zoning in place, Potterytown (CD) could be built above at a higher elevation than the trail – need overlay district.
 - c. The wooden trestle is beautiful, has tourism potential, and will bring national attention to this trail.

- 6) In the future, is it your desire to expand the scope or "Connectivity" of this Trail / Greenway? And if so, how?
 - a. Yes, of course. Refer to N Oconee River Greenway Master Plan and it's planned future connections to and eastern terminus at the Park-n-Ride which connects to UGA via Hardin Tract, route connection to East Athens Park currently underway, and eventually to Winterville and possibly Union Point.

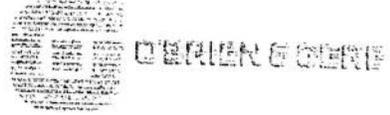
- 7) When Visiting similar trail features, who do you attend with and what activities do you participate in?
- a. Visit with family, friends, alone
 - b. Activities include: biking, walking, running, rollerblading

General Comments:

The railroad designed and built the corridor, all we need to do is restore it for bicycle and pedestrian use and get the funding to do so.

I believe that one of the primary reasons that O'Brien & Gere prevailed in the project proposal process was their overall positive and enthusiastic attitude toward the idea of keeping all possible existing (onsite) bridge components intact, and doing everything possible for adaptive re-use...that is, to incorporate those elements into the living trail versus making them interpretive "look-at" features. There was a stated commitment to "honor the bridges". Bill Doran's comment that "the bridges ARE this project", I think, rang true with the members of the RT committee who have made their own 7-year (to date) commitment to attempting to re-build these bridges almost completely lost – and to re-gain this level-grade trail. I hope very much to see OB&G honor their statements. The bridges are indeed the project...one in particular.

Rail to Trail - The Georgia Railroad Project
Citizen Input Questionnaire
March 19, 2008



In order to plan for the future development of the Georgia Railroad Project, we need your help in the form of ideas and candid opinions. Please take a few minutes to thoughtfully answer the following survey.

1. **Project Goal** - Please describe briefly, in very general terms, what your vision is for the proposed trail

Restoration of historic corridor, including bridges, into functional attractive, and safe bicycling/walking facility

2. What are your top 3 positive impressions of the project site? **CONFUSING**

1. Dudley Park Trestles - Both
2. Steady-grade connection from downtown to Eastside
- 3.

... negative impressions?

- 1.
- 2.
- 3.

3. What are your concerns regarding the three bridge crossing locations? Are your concerns largely functional or is it your desire that any design emphasize historical accuracy? **X**

Utmost importance to preserve both historic structures and the exact topography/grade.

4. Do you have specific environmental or preservation concerns about any portion of the proposed site? Historical views...sights?

At current zoning, Atterytown (CA) could be built higher than the trail. Need overlay district

Why?

For John Davis

Rail to Trail - The Georgia Railroad Project
Citizen Input Questionnaire
March 19, 2008



OFFICE OF THE GOVERNOR

5. In the future, is it your desire to expand the scope or "Connectivity" of this section of Trail / Greenway? Yes If so, How/Where Along historic corridor through Winsterville to Union Point.

6. When visiting similar trails features, do you most often attend with... (circle)
a. Family b. Friends c. Group (hiking, birding, etc) d. Alone ... Do you e. Bike
f. Walk/Run g. Rollerblade

The work has already been done!
The RR designed & built the corridor;
all we need to do is restore it for
bikapped use, (and get the funding to do so.)

Please feel free to add as many questions, comments or ideas on the back of this sheet.

Please fill out and return this questionnaire survey this evening if possible. If you would prefer you can either mail or fax the survey to Bill Doran @ Doran & Karwoski, Inc., 215 Church Street, Suite 101, Decatur, GA 30030 or via email bd.d-k@mindspring.com

Rail to Trail - The Georgia Railroad Project
Citizen Input Questionnaire
March 19, 2008



In order to plan for the future development of the Georgia Railroad Project, we need your help in the form of ideas and candid opinions.
Please take a few minutes to thoughtfully answer the following survey.

1. **Project Goal** - Please describe briefly, in very general terms, what your vision is for the proposed trail

- To add to the existing N Oconee River Greenway network
- To provide a level and thus readily accessible transportation corridor (linking East Athens with downtown + multimodal center and ACC parks (Dudley, & Athens))
- To provide a visually exciting trail that becomes the focus - end hub - of the N Oconee Greenway network

2. What are your top 3 positive impressions of the project site?

1. Historic Georgia railroad wooden trestle crossing
2. N Oconee River crossing
3. Views of Athens downtown from Dudley Park

... negative impressions?

1. Tullgate Station - devastation of berm ^{+ area} between Peter + Poplar streets *
2. Feeling of diminishing "safety" as one approaches eastern terminus of trail
3. (likely will change for better with new Park.n. Ride and redevelopment that will eventually occur along Oconee Street)

3. What are your concerns regarding the three bridge crossing locations? Are your concerns largely functional or is it your desire that any design emphasize historical accuracy?

I have no particular concerns for the N Oconee and Peter Street crossings - and while I like the idea of historical accuracy, I don't think that either of these crossings need to necessarily reflect their former selves (ie, not worth the extra expenditure). (and pretabs could look great). But for the historic wooden trestle, my concern is that it stay intact, that a commitment be made to doing what is needed to preserve at least the remaining structure, whether it is found to be structurally re-usable or not. Concerned that it be incorporated into whatever solution is reached for that crossing.

4. Do you have specific environmental or preservation concerns about any portion of the proposed site? Historical views...sights?

The preservation of the historic wooden trestle - first and foremost - while the N Oconee crossing will (even with pretabs construction) be dramatic and likely oft-photographed, it will be the wood trestle that will be the focus, the jewel of the project, and will become a symbol for the city of Athens.

First - its beauty. Second - its historic qualities (as linked to Athens and the history of the GA RR). Third - tourism potential (REM link - which will be of considerable value to the city for at least a few upcoming decades). Fourth - trestle crossings will generate photos - travel features - publicity - and perhaps help feature this trail in the National RT network.

Visually, this is likely to improve significantly as trees are planted and grow - financially, this is going to be a killer for the trail as berms may need to be shored up.

* Wood trestles have been used and still exist all over the U.S. and Canada. Maintenance is clearly somehow do-able - and we need to figure it out.

Rail to Trail - The Georgia Railroad Project
Citizen Input Questionnaire
March 19, 2008



5. In the future, is it your desire to expand the scope or "Connectivity" of this section of Trail / Greenway? of course! If so,
How/Where Refer to N Oconee River Greenway Master Plan where many
future connections have already been mapped out: to Eastern Tennessee
at Park n. Ride which connects directly to USA (via Hardin tract); RT connection to
Atlanta Park (under construction); eventually to Winsterville (and possibly 38 mi to Union Point)
6. When visiting similar trails features, do you most often attend with... (circle)
a. Family b. Friends c. Group (hiking birding, etc) d. Alone... Do you e. Bike
f. Walk/Run g. Rollerblade

Comment: I believe that one of the primary reasons that O'Brien & Gere prevailed in the project proposal process was their overall positive and enthusiastic attitude toward the idea of keeping all possible existing (onsite) ^{bridge} components intact, and doing everything possible for adaptive re-use ... that is, to incorporate those elements into the living trail versus making them interpretative, "lookout" features. There was a stated commitment to "honor the bridges". Bill Doran's comment that "the bridges ARE this project", I think, rang true with the members of the RT committee who have made their own 7-year (to date) commitment to attempting to re-build these bridges almost completely lost - and to regain this level-grade trail. I hope very much to see O&G honor these statements. The bridges are indeed the project ... one in particular!

Please feel free to add as many questions, comments or ideas on the back of this sheet.

Please fill out and return this questionnaire survey this evening if possible. If you would prefer you can either mail or fax the survey to Bill Doran @ Doran & Karwoski, Inc., 215 Church Street, Suite 101, Decatur, GA 30030 or via email bd.d-k@mindspring.com

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 000

Rails to Trails Project

Please print responses.

Name John Devine

Address 1263 East Broad St. 30601

Do you support the project? For Against Conditional Uncom

Comments I whole heartedly support the RT itself; the c
my support involves keeping the existing structures over T
the North Oconee R. - either using them or making them
be used. At NO point should the project compromise
RR topography - no dipping down to save money.
other grade levels

How did you hear about this meeting? Radio Newspaper Signs Wo
only heard today - should
have used the NRI. Other

Was the location of the meeting convenient for you to attend? Yes

If no, please suggest a general location that is more convenient to your communit

Was the time of the meeting convenient for you to attend? Yes

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes

Do you understand the project after attending this meeting? Yes

Please share your suggestions on improving the way Georgia DOT conducts public m
More ~~public~~ advertizing would be helpful - I haven't tal
anyone who knew about this before today.
Explain what will happen with the comments - wll
citizens should take the time to attend - as in, Ma
our concerns will be heard.

Mail To:

Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
Georgia Department of Transportation
3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Nathan Horne
Address 735 A Boulevard, Athens, GA
30601

Do you support the project? For Against Conditional Uncommitted

Comments Please complete!

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Mail To:
Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
Georgia Department of Transportation
3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 000756

Rails to Trails Project

Please print responses.

Name Larry Atkinson

Address 2335 Oakland Ave
30606

Do you support the project? For Against Conditional Uncommitted

Comments Actually, I support any rail to trail conversion

How did you hear about this meeting? Radio Newspaper Signs Word of mouth Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings

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Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
Georgia Department of Transportation
3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Peter Norris

Address P.O. Box 602
ATHENS GA 30603

Do you support the project? For Against Conditional Uncommitted

Comments THE PROJECT IS VITAL TO ATHENS' TRANSPORTATION PLAN, IT WILL PROVIDE FOR ECONOMIC STIMULUS (TOURISM) WHILE ALLEVIATING SOME OF THE TRAFFIC CONGESTION ON CONCER ROAK STREETS.

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth Other

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

PLEASE PROVIDE 7 DAY ADVANCED NOTICE TO PUBLIC & GOVERNMENTAL BODIES I.E. GREENWAY COMMISSION

Mail To:
 Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
 Georgia Department of Transportation
 3993 Aviation Circle
 Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Celeste Condit

Address 190 W. Interest Place

Winterville, GA 30683

Do you support the project? For Against Conditional Uncommitted

Comments it needs to be much longer! commute to
Winterville so I can commute to work safely

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Mail To:
Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
Georgia Department of Transportation
3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Daniel J. McGarvey

Address 115 Davis Estates Rd.

Athens, GA 30606

Do you support the project? For Against Conditional Uncommitted

Comments This Rails-to-Trails conversion is a terrific step forward for Athens. The current lack of pedestrian friendly infrastructure is one of the most glaring deficiencies in this otherwise exceptional community. Hopefully, this project will facilitate further support for similar projects & enhance

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you. 7 pm - 8

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings:

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Georgia Department of Transportation
Public Information Open House Comment Card
Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561
Rails to Trails Project

Please print responses.

Name James Corcoran
Address 165 Oakridge, Athens 30601

Do you support the project? For Against Conditional Uncommitted
Comments Anything to improve pedestrian & bicycle traffic is desperately needed!

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other listserve

Was the location of the meeting convenient for you to attend? Yes No
If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No
If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Mail To:
Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
Georgia Department of Transportation
3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation
Public Information Open House Comment Card
Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561
Rails to Trails Project

Please print responses.

Name Bruce Raulsback
Address 190 Wintersett
Winsterville GA 30683

Do you support the project? For Against Conditional Uncommitted
Comments We look forward to completion of the
project for both commuting & pleasure. Run it
to Winsterville!!!

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other

Was the location of the meeting convenient for you to attend? Yes No
If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No
If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

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Public Information Open House Comment Card

Project CSHP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Brent Buice
Address 279 Yanah Ave
Athens, GA 30601

Do you support the project? [X] For [] Against [] Conditional [] Uncommitted

Comments We should have done this years ago! I'm so glad we're finally moving forward with this. It will beautify our city & increase physical activity, while also creating more equitable transportation infrastructure. Build more!

How did you hear about this meeting? [] Radio [X] Newspaper [] Signs [] Word of Mouth [] Other

Was the location of the meeting convenient for you to attend? [X] Yes [] No
If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? [] Yes [X] No
If no, please suggest a time frame that is more convenient for you. 5:30

Were your questions answered by the DOT personnel? [X] Yes [] No
Do you understand the project after attending this meeting? [X] Yes [] No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.
Announce them earlier than "same day" please.

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3993 Aviation Circle
Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Nina Butler / work: NEGRDC
 Address 163 Indale Ave / 305 Research Dr.
Athens GA 30606 / Athens GA 30606

Do you support the project? For Against Conditional Uncommitted

Comments Should the murrum trestle be deemed
structurally unsound, please come back to
the public with options for that crossing point

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

more outreach — get to neigh'd associations
w/ more time, ask local govt agencies to post
on their web site.

Mail To:
 Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
 Georgia Department of Transportation
 3993 Aviation Circle
 Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Mike Ely
 Address 1688 Prince Ave Apt 208
Athens GA 30606

Do you support the project? For Against Conditional Uncommitted

Comments PLEASE complete this project as soon
as possible. It's an excellent idea and I can't
wait to ride it. If there is any way, please
save and reuse the existing railway trestles.
They are great history and reduce waste.

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? very helpful Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.
More online information.

Mail To:
 Mr. Glenn Bowman, P.E., State Environmental/Location Engineer
 Georgia Department of Transportation
 3993 Aviation Circle
 Atlanta, GA 30336-1593

Georgia Department of Transportation
Public Information Open House Comment Card
Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561
Rails to Trails Project

Please print responses.

Name Chris Fleisher
Address 170 Woodberry Ct
Athens, GA 30605

Do you support the project? For Against Conditional Uncommitted

Comments I'm a bicycle commuter, and I think this is a great project.
I think the Murren Truette should be used if it is structurally sound. If I
can't support the actual trail, I think it should be preserved as a major
component of the trail.

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth

Other Numerous organizations sent out notices, but
Very late, arriving on the day of the meeting

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Mail To:
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Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Amy Andrews

Address 282 King Avenue

Athens, GA 30606

Do you support the project? [X] For [] Against [] Conditional [] Uncommitted

Comments I'm tremendously excited about it! This is an excellent idea. Investing in alternative transportation routes is absolutely essential. We are going to remain relevant in our environmental and economic future. I would like to keep the "mormon" trestle intact and would like it is worth the money, it would like to preserve and/or rehabilitate it. A unique "sense of place" and is one of the keys to making this a successful and a

How did you hear about this meeting? [] Radio [X] Newspaper [] Signs [X] Word of mouth [] Other

Was the location of the meeting convenient for you to attend? [X] Yes [] No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? [X] Yes [] No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? [X] Yes [] No

Do you understand the project after attending this meeting? [X] Yes [] No

Please share your suggestions on improving the way Georgia DOT conducts public meeting

Mail To: Mr. Glenn Bowman, P.E., State Environmental/Location Engineer Georgia Department of Transportation 3993 Aviation Circle Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Wray Witten
Address 224 1/2 Hill St.
ATHENS, GA 30601

Do you support the project? For Against Conditional Uncommitted

Comments Absolutely! Great way for pedestrians and b
riders to avoid traps, see a great piece of land,
get to Old Winderle Rd. Historic markers could be
added for the bridges.

How did you hear about this meeting? Radio Newspaper Signs Word of M
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

You might think about East Athens Community Cen
ter you excluded bus users from the "meeting".
Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes + No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings

The maps were terrible! Out of date base maps, with no
current buildings around Bus Station shown. Multimodal
bus station drawn on wrong location! (DOT remarks in
to question about map inaccuracies did not properly be
multimodal.

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Rails to Trails Project

Please print responses.

Name Beth Gavrilles
Address 233 Dubose Ave -
Athens, GA 30601

Do you support the project? For Against Conditional Uncommitted

Comments Most important to me is that the rail-~~s~~ remains at the original, existing, railroad grade if at all possible, use the "Murren" trestle, even it increases the cost. It will be a ~~big~~ draw to tourism (although I realize this is primarily for ~~tr~~ ~~corridor~~ rather than recreation, but it can do both.)

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings
Earlier notice of the meeting would have been helpful.

Mail To:
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Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name DAVID E. MELANCON

Address 165 A Pamela Dr.

Athens GA 30605

Do you support the project? [X] For [] Against [] Conditional [] Uncommitted

Comments It is a good beginning for the State. As more federal/state/local funds become available I hope to see more. I loved the migration with ACC Greenway.

How did you hear about this meeting? [] Radio [] Newspaper [X] Signs [] Word of Mouth [] Other

Was the location of the meeting convenient for you to attend? [X] Yes [] No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? [X] Yes [] No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? [X] Yes [] No

Do you understand the project after attending this meeting? [X] Yes [] No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Three horizontal lines for providing suggestions on improving public meetings.

Mail To: Mr. Glenn Bowman, P.E., State Environmental/Location Engineer Georgia Department of Transportation 3993 Aviation Circle Atlanta, GA 30336-1593

Georgia Department of Transportation

Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Elizabeth Little

Address 170 N. Homewood Dr
Athens, GA 30606

Do you support the project? For Against Conditional Uncommitted

Comments This would provide a vital link for alternative modes of travel into town. I would like to see the re-use the trestle rather than demolition. Hopefully, the public can see the cost associated with preservation vs. repla

How did you hear about this meeting? Radio Newspaper Signs Word of mouth Other

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings

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Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Kavan Williams
 Address 2500 Westview Dr
Athens GA 30606

Do you support the project? For Against Conditional Uncommitted

Comments SAVE THE MORMON TRESTLE

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth
 Other _____

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

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Public Information Open House Comment Card

Project CSHPP-0007-00(561), Athens-Clarke County, P.I. No. 0007561

Rails to Trails Project

Please print responses.

Name Meredith Welch-Devine

Address 1263 East Broad St.

Athens 30601

Do you support the project? For Against Conditional Uncommitted

Comments I very strongly support this project and believe that it is a critical component of efforts to make our neighborhood more bikeable, walkable, and by extension, livable. I understand that the "Mummi" trestle may not be structurally sound, but I thoroughly support any and all efforts to preserve the aesthetic quality of the area and to avoid, as much as possible, elevation changes along the route.

How did you hear about this meeting? Radio Newspaper Signs Word of Mouth

Other Neighborhood list-serve

Was the location of the meeting convenient for you to attend? Yes No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? Yes No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by the DOT personnel? Yes No

Do you understand the project after attending this meeting? Yes No

Please share your suggestions on improving the way Georgia DOT conducts public meetings.

Publicize through more channels earlier - I just found out about it today.

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Atlanta, GA 30336-1593

Public Outreach



- comments
- summaries
- projects
- open houses
- e-mail
- categories
- reports
- users
- documents
- help
- cheat sheets

Comments

- review
- add
- update
- delete

CommentReview

Comments Pending Responses for Project ID: 0007561
Open House Date: 11/18/2008



First Name: Mabel Last Name: Atkinson
 E-Mail: scottica@charter.net
 Address: 345 Red Fox Run
 City: Athens State: GA Zip: 30605
 Category: General
 Comment: Save and use the Murmur Trestle if at all possible. It will add distinction and uniqueness to this Rail-Trail project. I look forward to using this trail to bike downtown in the future.

- return
- edit
- respond now
- forward

The GDOT offices below are asked to review the responses provided by the consultant for the comments in their section. The project manager will review all responses.

REVIEWING OFFICE	COMMENT #	NATURE OF COMMENT	PROPOSED RESPONSE
Design	1, 12, 13, 14, 16, 17, 18, 19, 20	concerned with maintaining existing alignment & grades, and utilizing the existing structures crossing the North Oconee & Trail Creek	The existing vertical and horizontal alignment will be maintained in the design of the new trail. The existing structures are being evaluated for structural integrity for their proposed new use (i.e. no longer supporting a rail line, but a pedestrian path with max capacity for emergency vehicles). Every effort will be made to keep these existing structures, but the cost to rehabilitate the structures, the cost of maintenance for this type of structure, and the environmental impacts from this kind of structure are part of the evaluation.
	10	suggested historic markers	Historic markers are planned for the trail where appropriate.

REVIEWING OFFICE	COMMENT #	NATURE OF COMMENT	PROPOSED RESPONSE
Planning	5, 8, 16	would like the trail to be longer	The project limits have been set. In the future there is a plan to eventually extend the trail to Winterville. But it is not part of this project.

REVIEWING OFFICE	COMMENT #	NATURE OF COMMENT	PROPOSED RESPONSE
All	2, 3, 4, 6, 7, 9, 15	in support of the project	<p>Thank you for your input regarding the public information meeting for the proposed project. Your interest in this meeting and your comments are appreciated. Your comments will be made a part of the official record of the project.</p> <p>The attendees of the open house and those persons sending in comments afterwards raised the following questions and concerns. The GDOT has prepared one response to all comments so that everyone can be aware of the concerns raised and the responses given. Please find the comments, concerns, and questions listed below along with their response.</p>
	11	would like to have another public meeting if the Murmur Trestle is structurally unsound to see the other options	There is not another planned public meeting, but we will be meeting with user/interest groups with the alternatives for the trestle.

Attached is a complete transcript of the comments received during the comment period and a copy of the public information open house handout for review. **Your input on the proposed responses is required by December 19, 2008.** Please direct your comments via email to kcoley@dot.ga.gov (Kim Coley) of the Gainesville District One Office.

If you have any questions about the comments, please either email or call Kim Coley at (770) 532-5582.

GB/kc

Attachments

DISTRIBUTION:

Genetha Rice-Singleton, Asst. Director of Preconstruction

Billy Cantrell, w/attachments

District Engineer (Attn: Russell McMurry, P.E.), w/attachments

Angela T. Alexander, w/attachments